

WARD: Lockleaze

SITE ADDRESS: Land On South Side Of Bonnington Walk Bristol

APPLICATION NO: 20/02523/FB Full Planning (Regulation 3)

DETERMINATION DEADLINE: 11 September 2020

Redevelopment of site to provide 185no residential dwellings (Use Class C3) with vehicular access from Bonnington Walk and Landseer Avenue. Provision of community space/sales suite, car and cycle parking, refuse and recycling storage, hard and soft landscaping together with new and enhanced areas of public open space, children's play space and allotment provision.

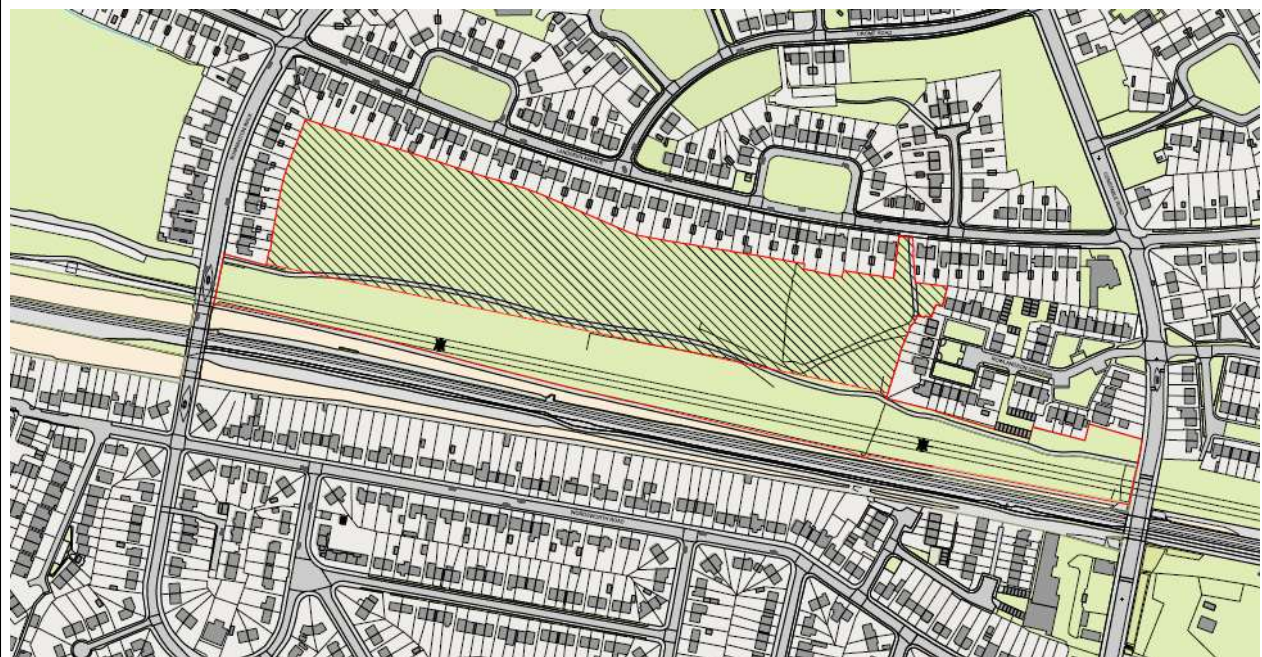
RECOMMENDATION: GRANT subject to Planning Agreement

AGENT: CSJ Planning Consultants Ltd
1 Host Street
Bristol
BS1 5BU

APPLICANT: Bristol City Council
C/o Agent

The following plan is for illustrative purposes only, and cannot be guaranteed to be up to date.

LOCATION PLAN:



Development Management – 25th November 2020**Application No. 20/02523FB: Land on South Side of Bonnington Walk Lockleaze Bristol****SUMMARY**

This application is presented to Committee following a members briefing held on October 28th 2020 and an informal site visit (at the time of writing scheduled) this morning. Proposals meet many City Council key strategic objectives. The scheme is judged to be a sustainable form of development, in all its forms, to include biodiversity, ecology as well as energy efficiency. Importantly it brings with it a Net Gain to Biodiversity (BNG) for the site as a whole, (a term defined in the forthcoming Environment Act). The site is allocated for housing, BSA0402 refers, but the incursion of development into the Site of Nature Conservation Interest (SNCI) is not so designated. The Nature Conservation Officer remains (in part) opposed to proposals for this reason. Natural England has no objection

The provision, in large part, through the site of an enhanced segregated 5m shared cycleway/footpath, on the Concorde Way provides at once for a safe route and environment of strategic importance. In their revised form proposals enhance the visual impact of the development. With greater opportunity to both protect and enhance wildlife habitat than originally intended. The layout protects the living environment for neighbouring residents and meets the needs of future occupiers. A policy compliant level of affordable housing (a minimum of 30% with an intention to deliver 50%) is intended with new open space and landscaping. Among the other obligations are; Ecology, tree planting, traffic management, public realm, and open space. Appropriate provision is also made for new allotments (reprovision elsewhere). The community orchard is to be maintained on site. Should members support the resolution to grant planning permission officers seek delegated approval to complete the terms of the necessary s106 agreement and a full schedule of condition(s).

SITE DESCRIPTION

The Bonnington Walk site as a whole measures a total of 6.29 hectares (that includes a no Build Zone), with a developable site area of 3.73 hectares. The site is roughly rectangular in shape and bordered to the west by the railway and embankment, which sits at a lower level than the site. To the east are the rear gardens of semi-detached houses that front onto Landseer Avenue. To the north are the rear boundaries of residential properties fronting onto Bonnington Walk. On the southern boundary are the rear gardens of properties on Rowlandson Gardens, and in the south western corner a parcel of land that extends south to meet Constable Road.

The site is owned by the City Council and allocated for housing- Site Allocations and Development Management Policies DPD document (Adopted July 2014). Within the site boundary and to the west of the housing allocation is land that is designated as Important Open Space. Rowlandson Gardens and Land at Bonnington Walk; ISO120 refers. This Open Space designation stretches beyond the confines of the application site to run south of Constable Road and Lockleaze Road, to include the Dover court Road small holdings; which have separate road access from Dover court Road (the Dover court small holdings are also owned by the City Council and are the preferred site for reprovision of allotment plots).

The site has a limited number of existing points of access / frontage onto adjacent highway. Access includes a gate from Landseer Avenue, and the accesses to the Concorde Way from Bonnington Walk and Constable Road. Land adjacent to the west is allocated as both a Citywide Site of Nature Conservation Interest and Open Space. This land is also allocated as a Greenway as the Concorde Way linking the City Centre to the North Fringe. The Site of Nature Conservation Interest (SNCI) area borders approximately two thirds of the western boundary and itself covers an area measuring 1.02ha. Beneath the site are underground electricity cables. To the west of the application site, beyond electricity overhead pylons and located in a deep cutting, is the main railway line between Bristol Temple Meads and Bristol Parkway/South Wales, and beyond this housing on Wordsworth Road.

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Among many replies to public consultation is representation that refers directly to potential errors made in the Arboricultural survey that accompanies the drawing(s). The suggestion made is that the precise number of trees currently on the site is underestimated, in part because of a failure to record in sufficient detail the character of tree groups. At the time of writing this issue remains the subject of an ongoing discussion with the landscape team- as does the intended incidence of tree felling and replanting- to include the character and distribution of each- (see description of works below).

To date the survey has identified 40 existing tree group(s) and 58 individual tree(s) on the site; approximately 250 trees in total. The existing tree groups are to be found spread across the site as a whole and the individual trees tend to be concentrated in the south eastern corner of the site. There are no TPO trees on site and there are none that are currently identified as A grade trees. The survey states that there are 7 individual and 2 tree groups classified as U grade, (unsuitable for retention). 4 out of the 7 U grade trees are Lombardy poplar trees (see survey T53, T54, T55, and T64). There are a further 5 Lombardy poplar trees due to their height and shape, are currently graded as C (T56) and B (T46, T47, T60, T62) refer.

Relevant Planning History

The site was acquired by the City Council incrementally between 1914 and 1939 together with further land holdings including the land parcel subsequently used to construct neighbouring properties in Rawlinson Gardens. Part of the land has previously been used for the grazing of horses, although this use has ceased on any regular basis. Part of the land is still used as allotment plots, this use was once more wide spread across the site but has also now very largely ceased.

In 2007 a resolution to grant outline planning permission (on part of the site) for 154 homes was considered by the (then) planning committee. A decision was taken at the time to defer the scheme and the site did not (then) come forward. In November 210 planning permission 10/03887/FB was granted for construction of a 3 metre wide pedestrian and cycle facility from Constable Road in Lockleaze North, to Bonnington Walk, through the Bonnington Walk Open Space.

Most recently some site clearance and translocation of protected species in particular slow worms has taken place-these works have been completed in advance of any grant of planning permission in part because of the need to complete works in the requisite season

Screening Opinion-Environmental Impact Assessment- EIA.

A request for a screening opinion as to whether an Environmental Impact Assessment (EIA) is required to build up to 245 dwellings with associated access was made in June 2019. A written response dated 21st August 2019 – confirmed that an EIA is not required in conjunction with development.

EQUALITIES ASSESSMENT

The public sector equalities duty is a material planning consideration as the duty is engaged through the public body decision making process. "S149 of the Equalities Act 2010 provides that a public authority must in the exercise of its functions have due regard to:-

- (a) Eliminate discrimination, harassment, victimisation and any other conduct prohibited under the Act
- (b) Advance equality of opportunity between persons who share a relevant protected characteristic and persons who do not share it
- (c) Foster good relationships between persons who share a relevant characteristic and those who do not share it.

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During the determination of these proposals due regard has been given to the impact of the scheme upon people who share the protected characteristics of age, disability, gender reassignment, marriage and civil partnership, pregnancy and maternity, race, religion or belief, sex and sexual orientation. In their assessment of these proposals your officers are satisfied that any adverse impacts can be addressed and mitigated through the detailed design of the buildings and the imposition of appropriate conditions

Proposals

Development of the site would provide a total of 185 residential dwellings in a mix of 1, 2, 3 and 4 bed houses and apartments. Areas of open space/amenity space, together with new vehicular, cycle and pedestrian links to the surrounding neighbourhoods are also intended. The chosen modular method of construction would be employed to complete two storey houses with apartments set out in 3 and 4 storey blocks. Four disabled access units to the north east of the site would be constructed using traditional methods. A new community building is also intended at the southern end of the site.

Two new vehicular access points are intended, one from Bonnington Walk to the north and a second via Landseer Avenue to the south east. In addition to the two vehicular access points, pedestrians would also be able to link onto the Concorde Way cycle path which runs north south to the west of the site through the existing open space. Car parking would be provided predominantly on street but allocated to individual plots throughout the development. The current layout proposes 202 vehicle parking spaces, Electrical charging point would be provided for 20% of the spaces. A single car club parking space is also intended.

Trees

In their current form proposals involve the removal of 20 individual trees, 16 full tree groups and parts of four tree groups. This includes 7 individual trees classed as moderate quality (Category B), with the remaining features classified as low quality (Category C). No category A trees are proposed to be felled. 455 trees are to be replanted. These are to comprise a total of 400 planted on site, with provision then also made for an additional 55 new trees off site- a net increase of new 161 trees overall.

Affordable Homes

A total of 50% of the new homes would be affordable with an intended tenure split of 69% social rent and 31% shared ownership. 30% (55 dwellings) intended for social rent. This would be made the subject of a Section 106 Agreement. An additional 20% of new homes beyond the scope of the s106 (38 dwellings) would comprise a mixture of social rent and shared ownership.

Following registration of the scheme in June the following changes to the scheme have been presented. The most recent of these changes, appears in drawing(s) received on 10th November 2020.

1. Removal of the intended new allotments (to be re provided off site- see obligations)
2. Concorde Way cycleway widened to 3m with separate 2m wide pedestrian footpath with a pinch point- approximately 10m in length where the path narrows to 2.6m-to include relocation of the Community Orchard.
3. The pavement width has been increased and tiger crossing to Bonnington Walk road junction at north end of site to provide the requisite space for a 3m cycleway and 2m footpath (5m wide shared space overall). The road carriageway has been narrowed.
4. The central perimeter residential block rear gardens have been amended to limit access from the rear paths

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The scheme provides for the following s106 planning obligations

Affordable housing: 30% on site.

Traffic and transport; Contribution towards the installation of a MOVA system at the Filton Avenue/Toronto Road/Bridge Walk signalised junction- £35,000.

The installation of 2 new bus stops on Bonnington Walk- £32,845.

TRO: 2 x TROs (to control parking on paved areas and to ensure clear access for all vehicles, and for waiting restrictions on Bonnington Walk and Landseer Avenue required to protect access) - £11,826.

Travel Plan: Implementation of travel plan - £26,640

Tree replacement: - £42,086.55 (£765.21 x 55 trees)

Fire hydrants: 5 x new hydrants within the development - £7,500 +VAT

Allotments: contribution towards offsite allotment provision –TBC.

SNCI – commuted sum for long term management – TBC

Stockholm Tree Pits- TBC

Open Space- TBC

The supporting documents include the following:

- • Hard and soft landscaping schemes,
- • Design & Access Statement,
- • Landscape Design & Access Statement,
- • Affordable Housing Statement,
- • Planning Supporting Statement,
- • Planning Obligations Statement,
- • Air Quality Assessment,
- • Tree Survey Report, • Arboricultural Impact Assessment,
- • Historic Environment Desk-Based Assessment,
- • Phase 1 Environmental Site Assessment,
- • Noise and Vibration Assessment,
- • Health Impact Assessment,
- • Operational Waste and Recycling Management Strategy,
- • Preliminary Ecological Assessment,
- • Badger Report,
- • Bat Report,
- • Biodiversity Report,;
- • Botanical Report,
- • Breeding Bird Report,
- • Reptile Report,;
- • Invasive Non-Native Species Report,;
- • Community Involvement Statement,
- • Sustainability Statement,
- • Energy Statement,
- • BREEAM Communities Statement, including Trackers,

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- • Building Overheating Analysis,
- • Flood Risk Assessment,
- • Drainage Strategy,
- • Transport Statement
- • Travel Plan Statement,
- • Utilities Report, including Broadband Connectivity.

RESPONSE TO PUBLICITY AND CONSULTATION**Community Engagement**

This was completed prior to registration of the planning application in accordance with the City Council's approved Statement of Community Involvement, as well as NPPF guidance. A meeting was held on 5th September 2019 involving; the Lockleaze residents planning group, Neighbourhood Trust and ward councillors. A second event titled 'Our Lockleaze' was then held at the Cameron Centre on 28 September 2020.

Public Consultation on concept designs – then followed (between October to December 2019) to include

- • Public Consultation event at Lockleaze Housing Festival, The Hub, 26 October
- • Public Drop-in session at The Hub, 29 October
- • Feedback period 26 October to 11 November
- • Ecology site walk-around, 7 December

Further Public Consultation then took place on the (then) preferred design – in February 2020 to include;

- • Public Drop-in session at the Cameron Centre, 10 February
- • Landscape and ecology workshop, Lockleaze Sports Centre

Overall good public engagement was good with over 90 people attending the two rounds of consultation. 60 people submitted comments via the survey during the first round of consultation, and 17 responses were received during the second round. Responses made to the proposals overall were mixed. It is widely accepted that more mixed-tenure housing needs to be provided in the area and there is some support for developing housing on the Bonnington Walk site. Significant concerns were however raised at the start of the consultation process particularly regarding the extent of development, loss of green space and impact on wildlife. There were also some issues raised regarding the ability of local infrastructure to support the development and traffic management.

The Lockleaze Residents Planning Group (including members of the Lockleaze Neighbourhood Trust and the ward councillors) recognises the need for housing and wants to see this balanced with protecting wildlife corridors and sites of nature conservation interest (SNCI) as well as supporting health and wellbeing for Lockleaze residents. The group welcomed the reduction in the number of homes and the creation of green corridors around the perimeter of the site.

Lockleaze Neighbourhood Trust submitted a separate additional written response offering to manage and curate the proposed community space within the development. The Trust offered a number of more general observations and suggestions regarding development across the wider Lockleaze area, in relation to use of solar energy, transport planning, protecting wildlife / supporting biodiversity, and public art.

Positive comments focused on:

- • Principle of housing on the site

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- • Support for affordable/social housing
- • Retaining the SNCI and keeping some open space
- • Keeping the cycle path and the community orchard

The main concerns were:

- • Loss of open space and trees
- • Destruction of wildlife habitat/corridor
- • Density of the new housing
- • Privacy of existing residents
- • Access to the site and more generally to Lockleaze
- • Increased traffic impact on surrounding road network including parking
- • Proximity of pylons
- • Impact on local infrastructure/services e.g. schools, GP surgery, etc.

Formal Registration

The planning application itself was then registered with the City Council on 12th June 2020. The related formal period of public consultation ended on 16th October 2020. This included Site Notice(s), Press advert and two separate three week periods of neighbour consultation, the first in July and the second following revision of the scheme, beginning in September.

At the time of writing a total of 131 replies have been received this includes correspondence received both prior to and after the formal consultation period ended. Representation received from Lockleaze Cllrs Gill Kirk and Cllr Tincknell who both support the scheme is set out below.

In summary and at the time of writing, 124 oppose replies scheme and 4 offer their support to it. (Some householders have written more than once). Objection(s) can be summarised as follows:

Loss of open space

Loss of trees

Detriment to health, noise, air pollution, traffic

Detriment to flora and fauna

Detriment to wildlife habitat

Loss of opportunity for and failure to achieve climate change objectives

Detriment to road safety

Loss of privacy

Development is judged to be out of character

Increase traffic and congestion

Detrimental Impact upon sunlight to neighbouring gardens

Lack of (or insufficient) public infrastructure

Insufficient parking proposed.

Increase(s) to antisocial behaviour.

Increased flooding

Increase pollution

Allotments off site; is judged unacceptable removing opportunity for Community engagement and for improved mental health and wellbeing.

Detriment to health by reason of proximity of new homes to Power lines-(this includes commentary from the Lockleaze Environment Group stipulating particular medical condition(s)).

Natural England has no objection to the scheme and comment as follows;

“The proposed development is within an area that Natural England considers could benefit from Enhanced green infrastructure (GI) provision. Multi-functional green infrastructure can perform A range of functions including improved flood risk management, provision of accessible green

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Space, climate change adaptation and biodiversity enhancement. Natural England would encourage the incorporation of GI into this development”.

Network Rail has no objection to the scheme and if planning permission is obtained, as immediate neighbours, would welcome early engagement over varied issues

In their statement the **Bristol Cycling Campaign**, responding to the initial scheme, objected on the grounds that the development did not (then) make adequate provision for the Concorde Way cycle route.

The **Cycling Campaign** stated

“Southern section - width. We are disappointed that no upgrade is proposed to the southern section of the cycle route. Our concern is that the separator between these two modes will not be adequate. We recommend at minimum a 0.3m grass or planted strip between the two paths so it is clear for all users. The proposed seating will effectively narrow the usable width. This section should be upgraded to the same 3m + 2m standard as the northern section. If there are width constraints, the cycleway could be routed behind the Allotments and orchard, alongside the railway line.

- *Bonnington Walk crossing - alignment. The crossing at Bonnington Walk is a difficult and Dangerous section of the Concorde Way route. We understand a parallel crossing is due To be provisioned here. The cycleway should align with this crossing point. The current Proposal of a 3m width shared path section is not suitable for an area which will have Conflicting cyclist and pedestrian movements.*
- *Bonnington Walk - connection to Landseer Avenue. Many cyclists leave the Concorde Way at this point to head east towards UWE. Provision of a separated cycle track along The short section of Bonnington Walk between the Concorde Way and Landseer Avenue Would vastly improve safety for cyclists heading in this direction.*
- *Constable Road crossing. No treatment is proposed for the crossing of Constable Road And access to the next section of the cycle route on Dover Court Road”*

(At the time of writing no second statement has been submitted in response to the subsequent change(s) made to the path).

Friends of Suburban Bristol Railways (FOSBR) consider

“The expedited delivery of Horfield/Lockleaze station (and Ashley Hill station) prior to construction of local housing would be preferable. New residents would be more likely to take up public transport options rather than defaulting to car use with potential associated Congestion”

Writing in July **Bristol Walking Alliance** stated

“In order to provide clear segregation, we ask that there is a difference in level between the walking and cycling paths, as planned for other segregated routes, such as along A4018 Passage Road. A difference in level provides a clear and familiar means of ensuring cyclists do not stray onto the footway and those pedestrians (especially those with visual impairment who use canes or other mean(s) of tactile guidance) do not stray onto the cycleway”

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The Bristol Tree Forum object to development

“The need to build housing to meet sustainable economic or social development objectives should not be allowed to take precedence over ensuring that the development is also both environmentally sustainable and meets Net Gain objectives.

Whatever the merits of this application of achieving its primary goal to provide much needed housing may be, it should not be permitted to proceed unless and until it has properly addressed how it will replace and build upon the Green Infrastructure (including trees) that will inevitably be lost if this application proceeds as presently formulated.

The existing trees have a significant asset value which should not lightly be ignored. Using CAVAT, we have valued them at £4,674,918.

Under the Mitigation Hierarchy, trees should not be removed unless there is no realistic alternative. One alternative would be to build around the trees rather than remove them”

Among those who **support** the scheme are **Lockleaze Councillors Gill Kirk and Estella Tincknell** who write as follows;

“There is a huge demand for new affordable housing in Lockleaze due to the demolition of council housing stock and its transfer to the private sector in recent decades. We therefore share the city’s ambition to tackle the housing crisis and support the building of affordable homes. This development proposes 50% affordable housing, and innovative modular housing design with grade A energy sustainability ratings and low running costs. We support a mix of tenures whereby people are able to settle in to a mixed balanced community and raise their families with various housing options available as their circumstances change. We have worked closely with the Lockleaze Residents Planning Group to pick up issues of concern and ensure good communication between the Council, developers and the local community regarding these proposals.

This has led to some significant compromises and mitigations, particularly on the site layout, wildlife habitat and tree planting. The number of four-storey blocks of flats has been reduced to two and will now be situated towards the centre of the site, to minimise visual impact. As a result, there will be more public green space around the buildings.

We welcome that this proposal offers a high level (50%) of affordable housing, including social rent and shared ownership, and would like this to be a condition of planning approval.

However, Bonnington Walk open space is a site that has been highly valued by local residents as a green urban space with mature trees, extensive wildlife habitat, and a community orchard, so there remain significant environmental considerations to take into account if the new development is approved.

The key issues of concern that remain for us include:

1) Community, traffic and transport infrastructure:

We wish to see a co-ordinated approach to this and other development sites in Lockleaze, looking at traffic and transport infrastructure, safe road crossings, speed calming measures, and sufficient parking.

Access to school places, GP surgeries, and the need for more amenities such as shops, cafes and an improved library should also be part of the process.

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We would like this to be a condition of planning approval.

(2) Loss of allotments from the initial designs:

*Food growing is a priority in the **Our Lockleaze Community Plan 2019-24**. Internal council objections have been raised to providing allotments on-site which has come as a huge disappointment to residents.*

We would therefore like this decision to be reconsidered, and the community enabled to play a role in managing these allotments. If the developers cannot provide on-site allotments, we would like to see a higher financial commitment to increase allotment capacity very nearby.

(3) The cycle path and protection of the Community Orchard:

We note that the widening and segregating of the path for cyclists and pedestrians is as a result of new government guidelines on cycling infrastructure. This change has impacted on the Lockleaze Community Orchard, which will now lose a strip of land.

We therefore seek a commitment to the re-provision of extra space adjoining the orchard that will enhance it, and create space for a community garden with raised beds and planters to encourage local volunteering and help build community cohesion.

We are also asking for a clear commitment to residents having input over the use of the community building on site, plus a commitment to providing tool storage for the orchard volunteers and some financial support for developing community volunteering.

(4) Tree retention and replanting/green perimeter and wildlife corridor:

*Regarding tree retention, we **object** to the number of mature trees being removed and would ask for a reassessment to enable more to be retained as long as they have long term viability.*

We would like as many trees as possible to be planted on the Bonnington Walk site, to make this a green landscaped area and to uphold the commitment to Bristol City Council's 'One City' Plan, which aims to double Bristol's tree canopy in the coming years.

We are pleased to see that designs have been altered to create an uninterrupted wildlife corridor along the western boundary of the site that will be protected and maintained as a Site of Nature and Conservation Interest.

We would like to see a firm commitment from the Council to managing the proposed wildlife corridor, to enhance management of the grassland and monitoring of habitats and biodiversity.

(5) Commitment to affordable housing and a local lettings policy:

We would like to see a firm commitment to the maximum level of affordable housing possible.

We also regard a local letting policy as an essential commitment. Local residents were assured by the former cabinet member for Housing, Paul Smith, that a local lettings policy would be supported.

We would therefore like these two commitments to be a firm condition of planning approval”.

Lockleaze Councillors Gill Kirk and Estella Tincknell

“Note: Officers Note: Should planning permission be granted payments allied to the Community Infra Structure, (CIL) monies would then be the relevant and appropriate mechanism that could be used to

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distribute funds to relevant local infra structure needs, including both Schools and Libraries beyond the site itself. The comprehensive Health Assessment that accompanies the drawings has been agreed to by Public Health Bristol and confirms that the Health needs of the new resident population approximately 384 could be reasonably be met via Horfield Health Centre. As a result there would be no necessity for any s106 obligation in this respect.

Among others supporting the scheme is the **Civic Society**.

Statutory and no statutory consultees

The **Nature Conservation Officer** has been involved from the onset in detailed appraisal of the scheme both prior to and following formal registration of the planning application (and continues to remain so as discussion continues over possible mitigation measures allied to any grant of planning permission). He remain(s) opposed (in part) to proposals; (see the consultation response upon the scheme as revised made 23rd September 2020 below)

In summary in monitoring progress of the scheme, through the course of both summer and autumn season(s) **The Nature Conservation Officer** has variously advised as follows;

On 27th July 2020 he wrote as follows;

*“The application site is included in the Site Allocations document, Site BSA0402, but the development of the SNCI area is not proposed within the Site Allocations document. Policy DM 19: Development and Nature Conservation in the Site Allocations and Development Management Policies document, states that: ‘Development which would have a harmful impact on the nature conservation value of a Site of Nature Conservation Interest will not be permitted.’ Accordingly and as previously indicated in my pre-application response **Object** to this planning application.*

*The proposal has employed Natural England’s Biodiversity Net Gain (BNG) biodiversity metric 2.0 (as amended) to develop ecological mitigation proposals and this is welcomed. However, the BNG assessment needs to show a positive figure, i.e. a biodiversity net gain and this is not the case. The BNG assessment shows a biodiversity net loss of minus 2.87% habitat units and so this does not meet national policy requirements. Accordingly **Object** to this planning application for this reason. Further off-site ecological mitigation is required and this needs to be calculated using Natural England’s Biodiversity Net Gain (BNG) biodiversity metric 2.0 (as amended).*

Accordingly the SNCI should be excluded from the development proposals to conform to Local Plan policy.

In addition a Section 106 financial contribution towards the ongoing nature conservation management of the SNCI should be provided because there will be additional recreational pressure on the SNCI resulting from the development.

The following comments should apply (if) this application is approved contrary to the objections above.

“A wildlife corridor (buffer) should be maintained on the site and this should include the retention of the Bonnington Walk SNCI to accord with policy as set out above and also the western boundary of the site which adjoins the Railway between Bonnington Walk and Constable Road Wildlife Corridor site.

Sufficient hibernacula need to be provided to meet the requirements of a high density of reptiles – 190 slow-worms from Crome Road have already been moved onto the site, with more being moved from within the site itself (peak count 16). This is a large population so it is particularly important that the

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carrying capacity of the receptor site is maximised for slow-worms.

If these changes can be made to the document prior to determination of the planning application then the strategy could in principle be conditioned as a compliance condition, otherwise a reptile mitigation strategy will need to be conditioned to secure details such as the number of hibernacula.

In the sown grassland areas I consider that the use of the Emorsgate EM4 or a similar seed mix should be considered instead of or as well as EM2. Any grass mowing will need to be undertaken in a way that avoids impact on reptiles especially in relation to the proposed regular mowing during grassland establishment. A method statement for this mowing should also be included within the strategy.

I recommend that the section of the SNCI strategy document that deals with site maintenance is expanded with respect to ensuring a ten year period of ecological monitoring of the development e.g. with further details of timings and actions. Please note the recommendation to install an interpretation board on the SNCI –if these changes can be made to the document prior to determination of this planning application then in principle the strategy could be conditioned as a compliance condition, otherwise a SNCI ecological management and monitoring strategy (nature conservation management plan for the SNCI) will need to be conditioned to secure details such as the number of hibernacula.

The site's landscaping also needs to be compatible with the requirements of reptiles and grassland conservation.

(Officers Note: Subsequently changes were made and discussion over the Strategy document is continuing. At the time of writing the translocation of reptile(s) has largely been completed under appropriate supervision).

On 23rd September 2020, the **Nature Conservation Officer** then wrote

“I'm pleased to see a positive Biodiversity Net Gain Assessment figure resulting from the revisions. I now remove my objection on this ground; (the current score is +4.48)

I maintain my objection relating to impacts on the SNCI”

On 12th October the **Nature Conservation Officer** stated with regard to any further intended changes to the scheme; (i.e. any that might be proposed before any Decision Notice was issued):

“If (any subsequent) changes are made to the landscaping that effect the Biodiversity Net Score (BNG) that has been calculated by changing the areas of habitats which are proposed, you will need to ask the planning agent to get the ecological consultants to re-calculate the BNG score accordingly to ensure that it remains a positive figure. This needs to be done prior to determination of the planning application”.

Relevant planning condition(s) (in addition to the required s106 agreement) are as listed in the recommendation.

A fully costed SNCI Strategy, to include appropriate mitigation measures is in preparation but the relevant commuted sum required for this purpose is not yet known.

The **Allotment team** have commented as follows:

“Due to the presence of multiple Western Power Distribution, (WPD), underground and overhead cables running through the site, the restrictions to planting imposed by WPD we consider it is not

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practical for allotments to be created at this location. WPD prohibit tree planting of any kind as they need to be 6m away from cables and even soft fruit must be 2m away, given the multi-cable across the width of the site this would preclude normal allotment cultivation.

We would therefore require an offsite contribution to develop allotments on nearby BCC sites and to upgrade allotments in the locality to increase the number of plots available and to improve facilities provided.”

At the time of writing discussion is continuing over the total commuted sum required but the final figure has not yet been agreed.

Writing in October the **Tree Officer** concluded:

*“In general, I am still concerned that the proposed planting density will require thinning (felling individual overcrowded trees) within 10 years, therefore reducing the total number of trees on site. The BTRS is designed to enhance local amenity by increasing the **number of trees** in the local area, rather than, for example planting a highly dense area of native species, which will be managed as a hedge in 10 years.*

I agree that the proposed copse planting will create a unique area with an enchanted woodland atmosphere; however this is not the purpose of the Bristol Tree Replacement Scheme. The BTRS is designed to secure meaningful tree planting with a contribution to local amenity for 50-100 years. I fear the copse will provide an attractive, short term (10-20 year) contribution to the local area, before competition within the copse creates dominant individuals, which cause weaker trees to fail and need felling. Replanting would be a challenge due to the density”

Officer’s note: At the time of writing discussion over the most recent intended changes to tree planting both on and off the site remains under discussion- members will be updated on the outcome(s). The Tree Officer has identified the need for condition(s) (as listed in the recommendation, these may be subject to change and others are likely to follow at a later date).

Traffic Development Management has confirmed that subject to completion of the requisite s106 agreement they have no objection to the scheme; subject also to the schedule of condition(s) as listed in the recommendation. In particular the team will require further details prior to any agreement upon formal adoption of highway(s). (At the time of writing discussion is continuing over the likely details to be allayed to these conditions). TDM write;

“There are a number of outstanding issues with the current submitted layout that will cause problems for any future developer when attempting to have the street layout adopted by BCC Highways. Whilst TDM are happy for these matters to be resolved pre-commencement (so as not to delay determination of the scheme), this will only be addressed satisfactorily following our insistence on a number of bespoke pre-commencement conditions, which are provided in this response.

It should also be noted the submitted Ownership and Management plan (19008-(00)_P005-P02) has not be agreed with TDM given the previous concerns raised regarding the acceptability of private parking between the adopted footway and carriageway. Therefore, this drawing cannot form part of the approved plans as the issues of maintenance; drainage, ownership and management have yet to be demonstrated to be acceptable to the highway authority.”

Highway Works

The applicant will be required to undertake the following highway works through an s278 highway works agreement:

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- *Installation of tiger crossing at Bonnington Walk including raised table*
- *Installation of tiger crossing at Constable Road including raised table*
- *Segregated two way cycle route of 155m in length from Dover court Road to the Constable Road National Cycle Network access*
- *Speed bumps along Bonnington Walk from Landseer Avenue to Wordsworth Road*
- *Widening of the access onto Landseer Avenue*
- *Creation of access onto Bonnington Walk*
- *Resurfacing of footway along Bonnington Walk and Landseer Avenue*
- *Upgrade of street lighting at access points on Bonnington Walk and Landseer Avenue”.*

Thereafter required pre commencement condition(s) and regulatory conditions are then recommended (as listed in the recommendation);

The **City Design Group (CDG)** has commented in support of the scheme. In doing so they conclude as follows

“Landscape and Urban Design officers from CDG have reviewed matters and consider(s) the further information now provided addresses most of the issues that were raised earlier.

CDG refer to the tree officer’s comments in relation to the loss of trees on site and suitable replacement as per Bristol Tree Replacement Standard; then BTRS”.

CDG considers the scheme meets the planning policy considerations. CDG therefore supports the application and recommends the following conditions(s) (in addition to those to be required by the both the Tree and Ecology Officer’s) as a means to resolve the chosen building design aspects of the scheme.-

The **Sustainable Cities Officer-Air Quality** has commented as follows upon the consequence of construction related vehicle movement:-

“The slight adverse impacts predicted are associated with the temporary construction phase of the project and so will be relatively short lived. The applicant has stated that they are flexible and open to looking at opportunities to mitigate the impact from the light duty vehicle construction traffic predicted to cause the slight adverse impact on one Muller Road receptor.

As a result I do not object to the development proposal on grounds of air quality impacts but would recommend that dust mitigation measures be conditioned and that further discussions take place to attempt to reduce impacts on air quality of LDVs on Muller Road during the construction phase of the development.”

Flood Risk Manager has no objection. The principle of the drainage strategy is agreed. Conditions will be required; (at the time of writing discussion is continuing over the content to these and none are listed in the recommendation).

Avon and Somerset Fire Brigade has requested fire hydrants to be secured under section 106 Agreement. Avon Fire & Rescue Service has calculated the cost of installation and five years maintenance of a Fire Hydrant to be £1,500 + vat per hydrant

Public Heath Bristol has no objection and fully supports the content and conclusions to the Health Assessment presented with the drawing(s).

The **Sustainable Cities team** support the scheme subject to conditions- to include future proofing and monitoring. At the time of writing discussion is continuing with condition(s) yet to be agreed and these do not appear in the recommendation.

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The **Public Protection Team Land Contamination** has no objection subject to regulatory conditions.

The **Crime Prevention Design Advisor** asks why Secure by Design accreditation is not being sought for the scheme design.-(the principles are being adhered to but the client team has chosen to be pursuing matters that are not mandatory).

Pollution Control Officer (Noise) has no objections; subject to regulatory conditions

RELEVANT POLICIES

National Planning Policy Framework – February 2019

Bristol Local Plan comprising Core Strategy (Adopted June 2011), Site Allocations and Development Management Policies (Adopted July 2014) and (as appropriate) the Bristol Central Area Plan (Adopted March 2015) and (as appropriate) the Old Market Quarter Neighbourhood Development Plan 2016 and Lawrence Weston Neighbourhood Development Plan 2017 and the Hengrove and Whitchurch Park Neighbourhood Development Plan 2019.

In determining this application, the Local Planning Authority has had regard to all relevant policies of the Bristol Local Plan and relevant guidance.

KEY ISSUES

(A) IS THE PRINCIPLE OF RESIDENTIAL DEVELOPMENT ACCEPTABLE?

The longstanding designation of the site for housing BSA0402 refers means that the principle of residential development of the site is accepted. Two issues are of particular note.

Incursion into Site of Nature Conservation Interest (SNCI)

This is limited to the main vehicular/pedestrian access to Bonnington Walk. It is imperative that the impact of development is fully assessed in respect of the resulting impact upon both existing landscape and ecological designation(s). Matters of ecological and landscape interest, including the need for measures to substantially improve the Biodiversity of the Site are considered in the key issue(s) that follow.

Overhead High Voltage Lines and Underground Cables

Adjacent to the site, running the length of the green corridor and parallel to the neighbouring railway line are Western Power Distribution (WPD) electricity Pylons and their associated transmission lines. Beneath the site are underground cables. Concerns have been raised in consultation responses in relation to matters of health and safety as a result of proximity to the pylons and the cables. There is a 30m easement between the overhead pylons and the new homes. Western Power Distribution; who manage and use the infrastructure, confirm that they have no objection. The distances are all in accordance with recommended National Guidance. Necessary and appropriate access to all infrastructure; (for maintenance purposes) would be enabled. These safety matters have had a direct influence upon the intended location and incidence of tree planting on site notably within parts of the Greenway. Following advice from the allotment officer, allotments plots originally intended on site are no longer so. The terms of the long standing site allocation expressly includes the need to make appropriate re-provision of allotment plots. At the time of writing work is currently continuing upon the feasibility of using the neighbouring Dover Court small holdings site for these purpose(s). At the time of writing the requisite commuted payment remains under discussion.

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Public health and well-being;

The document entitled National Planning Practice Guidance (PPG) 5 was first produced in November 2016 and most recently updated in October 2019. It offers guidance on health and wellbeing. It covers both, the role of health and wellbeing in planning; and the links between them. In this instance many consultation responses refer directly to the loss of wildlife habitat, open space and adversity of the proximity of development to the overhead lines. Resulting detriment to the area of the Community Orchard; is also referred to. All have been acknowledged and addressed in revisions made to the scheme in consultation with the community at large. The guidance also states “*Authorities should also assess quality, quantity of and accessibility to green infrastructure*”, this too has been addressed with improvements made to both open space and wildlife habitat. With access to the same available via the Concorde Way with sustainable methods of transport, walking and cycling, the greater use of which will promote better health for those who engage in them.

For the reasons given the principle of residential development on the site is judged acceptable

(B) WOULD THE PROPOSAL SATISFACTORILY ADDRESS ECOLOGY, LANDSCAPE AND WILDLIFE ISSUES?

Ecology

Policy BCS9 seeks to ensure that the integrity and connectivity of the green infrastructure network is maintained/ enhanced. The loss of green infrastructure should only be allowed where it is necessary on balance, to achieve the policy aims of the Core Strategy, with appropriate mitigation for the lost green infrastructure assets required. Policy DM19 states that development likely to impact on habitat, species or features, which contribute to nature conservation in Bristol will be expected to be informed by an appropriate survey/ assessment of impacts, be designed/ sited in so far as practically and viably possible, to avoid any harm to habitats, species and features of importance.

The need to make satisfactory arrangement for safe and secure for entry for motorists; pedestrians and cyclists explains the need to make a single incursion into the SNCI, at the northern end of the designated area. If the site is to be developed at all officers conclude that the geometry of the site provides for no practical alternative course of action. In their formal response to consultation Natural England, as the governments advisors upon the Natural Environment, confirm they have no objection to the scheme. The remaining objection of the City Ecologist on these grounds, alone is both acknowledged and fully understood.

There are in this case wider benefits to the scheme. These include, but are not solely limited to, the provision of new affordable homes. They also include other ecological proven advantage(s). The intended and varied provision of each of these must also then be appropriately weighed in the balance. In his own detailed and ongoing appraisal of the scheme the City Ecologist has acknowledged this fact. He has agreed upon the current rather denuded condition of the SNCI and thereafter also identified a package of potential measures, including recommendations that if implemented could both enhance and thereafter maintain the habitat for the long term. The National Planning Policy Framework (2019) states; paragraph 170(d) “*that planning decisions should minimise impacts on and provide net gains for biodiversity*”. The proposals have been fully appraised using the Department of Environment Food and Rural Affairs- (Defra) / Natural England’s Biodiversity Net Gain (BNG) biodiversity metric. The tool appraises in each case full detail of the data, measurements and workings used to calculate the percentage BNG. When initially registered, in June this year, the use of the Defra Metric produced a negative score. Importantly revisions made to the scheme since registration has ensured a Net Positive BNG Score. (From -2.87 to +4.48) This compliance with emerging National policy in relation to forthcoming Primary legislation (the draft Environment Bill), is an important material consideration in this case. At the present time discussion is continuing upon the

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character and content to a fully costed long term strategy for enhancement and maintenance of the SNCI. On balance, with the proviso that later agreement is reached upon implementation of all these measures; a grant of planning permission is recommended.

Reptiles

Slow worm have been identified on every survey undertaken predominately recorded within the semi-improved grassland in the middle and north of the site. The resident slow worm population is categorized as 'Exceptional'. Appropriate translocation has now been undertaken.

Bats

The majority of bat activity within the site is attributed to common pipistrelle (*Pipistrellus pipistrellus*). Activity from other species, including noctule (*Nyctalus noctula*), serotine (*Eptesicus serotinus*), Leisler's bat (*Nyctalus leisleri*), *Myotis* sp., soprano pipistrelle (*Pipistrellus pygmaeus*) and long-eared bat was also recorded. Bat activity was highest within the centre of the site and to the north of the site within the SNCI and these areas is, therefore, considered to provide important foraging/commuting habitats for bats using the site.

Landscape/Trees

Policy states that the distribution and variety of open spaces throughout the city will be maintained and where possible enhanced. Sites which are important for nature conservation, recreation, historic landscape interest, landscape quality, visual amenity, community or outdoor leisure related uses, or providing setting or relief to the built environment will be protected as open space. It further states that development that has a significant adverse effect on the interest, use, amenity, character or accessibility of such open space will not be permitted.

Account will be taken of the retention and protection of existing natural features and habitats, and, where appropriate, the benefits of new landscape treatment which respects the local landscape character and is well integrated with the buildings and existing landscape features on and surrounding the site. The most significant trees included within the survey are of moderate quality (Category B) trees. Of particular landscape value are the large mature Lombardy Poplar(s) (T46, T47, T60 and T62 refer). Particular care has been taken to appraise the given justification for their felling, to include the compilation of a pressure test. The results of the pressure test have been both appraised and agreed to by the Tree Officer. The prevailing condition of each of the poplars means that the consequence of retaining them in situ whilst electing to build around them would be unduly prejudicial to both their health and resultant life span.

In assessing the scope for new tree planting on the site, very careful consideration has been given to the number, type (species) and location, and how they may best be employed throughout the new streetscape, in order that they enhance views into & out of the site. The City Council has declared climate and environmental emergencies, has committed to becoming carbon neutral by 2030 and is also committed to doubling tree canopy cover by 2046. When viewed as a whole this scheme is considered to play its own important part in meeting these objectives

This assertion is contested in many and varied consultation responses, both from individuals and community groups. The Bristol Tree forum refers in their representation to the utility of Capital Asset Value of Amenity trees (CAVAT); a valuation system designed for use as a management tool for large tree population(s). Following the completion of the Planning Obligations SPD and adoption of the Bristol Tree Replacement Standard BTRS the CAVAT system of valuation has been dropped as a method of appraisal directly related to planning applications. At the time of writing discussion with your officers is continuing over the application of the BTRS methodology to the site; the outcome to these ongoing discussions will determine the eventual total number of replacement trees; their species size

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and distribution; both on and off the site. This discussion is informed by and allied to both the distribution of the open space within the scheme. This is so because planting that is too dense can sometimes bring with it demands at a later date for felling at too early a stage, perhaps 20/ 30 years.

There is a need then to minimise the risk of new occupiers and those who neighbour the site experiencing the adverse consequences of any overcrowding that might be apparent if trees were poorly located. Both a long term and robust approach is actively being sought to ensure that the new tree(s) planted each has as an appropriate life span. The most recently submitted drawing(s), issued on November 10th, propose a reduction in the number (to 400) of new trees intended on site with provision then made for a further 55 new trees off site. This number of trees is agreed to and meets the BTRS standard. The varied and sometimes competing need to successfully accommodate street furniture of varied kinds, including street lighting, tree pits, vehicle parking and utility cabling all to an adoptable standard will also necessarily inform decision making. Whilst a planting schedule has been provided, the precise location size and species, though not the total number of trees provided, may necessarily be the subject of change.

A number of recommendations are made in supporting documents pertaining to the intended character of replacement planting, supplementary planting of hedgerows to increase connectivity and the need for planting of native species. In respect of the enhancement of retained habitats, the report(s) identify measures to create new wide and linear, rough grassland, scrub and tree habitats connecting existing habitat within and adjacent to the site. In respect of monitoring, the recommendations made in terms of protected species monitoring are acceptable, and these too would be conditioned as part of any approval.

(C) HAS THE PROPOSAL SATISFACTORILY ADDRESSED TRANSPORT, MOVEMENT, AND ACCESS ISSUES?

Chapter 9 of the NPPF state that it must be ensured that any significant impacts from the development upon the wider transport network (in terms of capacity and congestion), or on highway safety, can and would be cost effectively mitigated to an acceptable degree. Highways England has been formally consulted and raised no objection. As most recently revised Highways Development Management team (TDM) have identified the measures required to off-set the likely impacts of the proposal in terms of parking, access, highway safety, and public transport infrastructure. These are as listed in the description of works and would, if planning permission were granted, be the subject of any s106 agreement and an appropriate schedule of condition(s). Each of the contributions listed meet the statutory tests set out within Regulation 122 of the CIL Regulations (as amended) and the National Planning Policy Framework:

- Necessary to make the development acceptable in planning terms;
- directly related to the development; and
- fairly and reasonably related in scale and kind to the development

Policy BCS10 states that developments should be designed and located to ensure the provision of safe streets. Development should create places and streets where traffic and other activities are integrated and where buildings, spaces and the needs of people shape the area. Policy DM23 states that parking must be safe, secure, accessible and usable. It sets out the minimum requirements for cycle parking and refuses storage provision in new development. As revised the layout meets these tests.

Policy DM23 seeks to protect and where and when appropriate to enhance existing public rights of way. Evidence collated to date does not suggest any loss of use rights allied to any rights of access across the site.

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Traffic modelling undertaken to date demonstrates that without appropriate mitigation current proposals would significantly compromise the operational capacity of the Filton Avenue/Toronto Road/Bridge Walk junction during peak hour periods to the detriment of safety, air quality and accessibility by non-car modes of transport. Without mitigation the resultant additional delay at this junction would have a severe impact upon the local highway network causing an increase in congestion whilst simultaneously increasing journey times on local public transport services decreasing their utility. This would be particularly prevalent for journeys routing to the north of the site

Signals

Having assessed the submitted Transport Assessment TDM requires the agreed contribution towards the installation of a MOVA system at the Filton Avenue/Toronto Road/Bridge Walk signalised junction.

Concorde Way

The Concorde Way is a strategic cycle route and would, if planning permission were obtained and the improvements intended to it then made, likely experience an increase in the number of cyclists and pedestrians using it. This would be both an intentional and welcome benefit to the scheme. Since registration of the scheme change(s) have been proposed to the cycleway at southern end of site to widen it to 3m with separate 2m wide pedestrian footpath. The intended pavement width and tiger crossing to Bonnington Walk road junction at north end of site has also been updated to provide space for a 3m cycleway and 2m footpath (5m wide shared space overall). The road carriageway has been narrowed. These are each considered both an appropriate and proportionate response to initial concern(s) expressed in varied consultation responses from local residents and interested parties. The government places a very strong emphasis on seeking an increase in the incidence of walking and cycling in order to improve the health of the nation. These aspirations then chime readily with both National and local planning policies. The gradients intended to the path are acceptable. The Traffic management team has expressed some concern over the intended geometry of the path, and the feasibility of protecting some existing tree(s) on the route. This is most notably so in relation to the intended narrowest portion of the cycle path. The rationale for variation and reduction in the width of the cycle path (to an intended minimum of 2.6m -for a distance of approximately 10m) is to provide for a suitable means to retain existing tree(s) and habitat. The precise composition of the Concorde Way for much of its length through the site has been and, at the time of writing, continues to be, the subject of discussion. The final composition of the route as well as technical specification would each be the subject of planning condition(s). These would need prior approval, to ensure that an adoptable standard to the surface of the path is agreed upon, implemented and thereafter successfully maintained. The objective would be to achieve this outcome without prejudice to other important material considerations, to include both ecological and arboricultural interests, in the short, medium and longer term(s).

Northern Crossing Bonnington Walk

This will allow safe and adequate access to the local facilities and the employment centres to the North. In common with other works this would need to be completed prior to occupation of the development.

Car Parking

The 202 vehicles spaces intended is below the maximum permitted under the local plan standard (237 spaces) and judged satisfactory. The scheme meets access needs with a minimum of 12 disabled spaces. To future proof the scheme and help meet climate change objectives a total of 41 electric charging spaces are required with ducting installed to allow every space to become EV ready. Further details would be required via planning condition(s).

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The intended location and total number of cycle parking spaces is judged satisfactory. Visitor parking around the site can potentially be successfully incorporated within the proposed buildouts. Where possible visitor cycle parking should accommodate adapted bikes and bike trailers.

Drainage and Adoption

A range of Sustainable Urban Drainage Systems SuDS including bio-retention systems and tree pits are intended in the highway. The principle of their use is agreed to subject to further discussion and agreement upon their detailed design. Bio retention pods do represent a potential maintenance liability and there is an obvious need to minimise their risk of being struck by passing vehicles. An overall street lighting strategy has been presented and is agreed to in principle. Due care and attention to detail will be required to avoid conflict(s) between the competing need for space of street lighting and the incidence of new tree planting. At the time of writing at both intended points of site entry and elsewhere the ongoing appraisal of these issues is made upon the assumption that any relevant nearby parking spaces (including offsite bays) would be occupied by parked vehicles.

Refuse & Servicing

Active consideration is being given to provision of a central location for deliveries within the development for residents. If there is an opportunity to consolidate freight and thus reduce freight related trips for example an amazon locker style system it should be taken. Refuse stores for the houses will be situated towards the frontage. This is acceptable providing they are not situated on the highway.

Public Transport

The agreed financial contribution of £32,845 is required for installation of 2 new bus stops on Bonnington Walk, at the northern end of the development – between the proposed access for the development and Landseer Avenue.

Traffic Regulation Orders TRO's

TRO's will be required to; control parking on paved areas and to ensure clear access for all vehicles, introduce waiting restrictions in Bonnington Walk and Landseer Ave and to protect access in each case. The dedication of a Car Club parking place would be included in the same restraint. The roads surrounding the site are currently subject to a 20 mph speed limit. It is assumed that the same speed limit would apply within the confines of the site.

Travel Planning

A figure of £144 per dwelling (£26640) will be needed to be secured via s106.

Car Club

The car club space should have provision made for electric charging. This would be secured by condition to require evidence of a signed contract between both parties before discharge.

Construction Management

A construction management plan would be required prior to commencement. This would need to consider the incidence of freight related trips as well as any impact on pedestrians, public transport or cyclists by freight vehicles. The use of contractors that meet suitably accredited vehicle and driver standards i.e. Fleet Operator Recognition Scheme (FORS) or Construction Logistics and Community

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Safety (CLOCS) would be recommended. A survey of the prevailing condition of the surrounding highway would also be required prior to commencement on site. Again a suitably worded condition would be applied should permission be granted.

(D) FUTURE LIVING ENVIRONMENT

Policy BCS18 states that all new residential development should help support mixed and balanced communities. The proposed houses would each have an internal floor space above the specified minimum and meet the relevant local standards by national and local allied to bed space(s) and family homes. By reason(s) of distance and orientation the views (window to window) available from the intended houses will not be to the detriment of neighbouring occupier(s). The principle of new residential development is therefore supported.

The Urban Living SPD promotes the completion of dual aspect rooms, which assist natural light, choice of views and cross ventilation providing greater capacity to address potential overheating problems. Just 18 of the 185 units is single aspect. These are all to be found within the apartment blocks and each faces either east or west. This is one consequence of the chosen modular method of construction. Of the total development and is judged acceptable. The Pollution Control Officer has reviewed the noise assessment and allied to the likely incidence of noise from neighbouring rail and road and concluded matters can reasonably be dealt with by means of condition(s).

Proposed open space.

This would link directly to the adjacent Important Open Space and SNCI. The requisite standards are all met. The calculus (included in the Urban Living SPD) estimates the number of children likely to occupy any given number of new homes, identifying the possible incidence of differing age ranges; 0 – 4, 5 – 11 and 12 – 15 year olds. The calculation is based upon the number of children currently living in households in Bristol, with due account then also given to different housing tenures and sizes. Use of the formulae would assume a total of 119 children in occupation. The area of public open space, including the square outside the community centre and orchard garden behind the intended community centre when taken together comprise a total of 6086sqm. The intended volume and composition to the space is agreed to, i.e. its distribution and geometry. Further discussion will be necessary to secure agreement over arrangements to be made for future maintenance including, the computation of payments made. At the time of writing the sums involved are not yet known.

Wheelchair accessibility

Compliance with policy DM4 requires that two per cent of new housing within residential developments of 50 dwellings or more should be designed to be wheelchair accessible, or easily adapt able for residents who are wheelchair users. To meet this standard the development as a whole would need to provide 3.46 units as accessible. The submission confirms that Units 51 to 54 (4 units) would be fully wheelchair accessible.

Safety and Security

It is important that, where appropriate, development recognizes the issue of crime prevention and helps to create safer, more secure development. This principle may include deterrent effects of good design, layout and lighting. Concern has been raised from neighbouring occupiers in respect of each. It is desirable as a planning objective to create a development that is permeable in respect of pedestrian linkages. It is acknowledged that the varied aims and objectives of "Secure by Design" have been recognized in scheme design, but formal accreditation is not currently being sought. Accreditation is not judged essential. It will be necessary to ensure that all users encounter both a safe and secure environment. Detailed measures to further help ensure this will need to be considered as part of the detailed design to both public and private realm, and will be further consulted on with Avon and Somerset Police.

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Policy BCS21 of the Bristol Core Strategy states that development should deliver high quality urban design, and sets out the ways in which development should achieve this. Policies DM26, DM27, DM28, DM29 and DM30 of the Bristol Local Plan- Site Allocations and Development Management Policies document (SADMP) set out more specific design criteria by which developments will be judged. The City Design team welcomes the principle of development as at once an intelligent and appropriate response to the topography of the site and surrounding area. The team advice is that the overall success of the scheme will be more than routinely dependent upon successful integration of the landscape with the new architecture. Following extended discussion with officers the originally intended amount of hard surfaced roadway has been much reduced. The relocation of the apartment blocks fronting the primary road allows for much greater flexibility with the SUDS allowing for increased opportunities for landscaping and tree planting.

Any significant failure in this regard (particularly an absence of the intended street trees or their failure at any early stage) would be problematic and prejudicial in visual terms. Ongoing discussion over these issues and further clarification of intent is required in relation the intended Public Art provision.

A number of local residents have raised objections relating to the likely incidence of overlooking from the proposed four storey buildings, but the blocks of this height are to be placed in the centre of the site. As a result proposals would not result in unacceptable overlooking or significant impact on light to gardens beyond. All the buildings arranged around the perimeter of the site are of lesser one or two storey height. The existing houses along Landseer Avenue to the east and Bonnington Walk to the north are typically around 5.1m high to the eaves and 7.7m high to the roof ridgeline. The new houses would have generous 2.6m floor to ceiling heights and so would be taller than the existing neighbouring properties. The ridgelines of the three house types would each vary; 2 bed house: 9.325m, 3 bed house: 10.13m, 4 bed house: 9.175m Each of these new dwellings would clearly have a physical presence/ impact on neighbouring existing properties but their scale and massing would each be entirely typical of the suburban environment of which they would form part.

At the southern end of the site, neighbouring Rowlandson Gardens the minimum distance between homes would be 15m, any windows in the new façade at this location would use obscure glazing. The primary elevations to the new building both front and back would face east and west. To the north neighbouring Bonnington Walk where the minimum distance between dwellings is 20m the new properties would be single storey height to minimise incidence of over shadowing and overlooking. This mitigation is judged appropriate. The removal of permitted development rights for each of the new dwelling(s) on the site is judged appropriate. This would serve to safeguard the privacy and amenity of existing and future occupiers. Potential incremental change proposed at a later date to individual homes can then be afforded appropriate scrutiny.

Importantly the landscape strategy includes potential opportunities to provide for public art through – provision of play spaces, local food production that could be linked to community orchard, events and performances, landscape for learning, community garden.

Overall the live ability aspirations set out in the City Council's Urban Living SPD are well rehearsed. The design advice offered in the longstanding document Bristol Local Plan Policy Advice Note 15 titled "Responding to Local Character is sufficiently accounted for. For the reasons given proposals would comply with to Policies BSCS20, 21, DM26, DM27, DM29 and DM30.

(F) DOES THE PROPOSED DEVELOPMENT ADEQUATELY MEET OBJECTIVES OF SUSTAINABILITY AND CLIMATE CHANGE?

Policies BCS13 to BCS15 of the Bristol Development Framework Core Strategy forms a suite of

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planning policies relating to climate change and sustainability. It requires development to both mitigate and adapt to climate change. This includes new development to minimise its energy requirements, address issues of sustainable design and construction and also water management issues to reduce surface-water run-off.

Proposals have been appraised using the Government's Standard Assessment Procedure (SAP). The consequent CO₂ results have then been assessed using an aggregate approach across the site, taking into account the orientation, number, size and type of dwelling including their modular means of construction. Energy needs for both Heating and hot water would be met via a combination of air-source heat pumps with PV panel(s); PV's would be located upon both the apartment buildings and houses. ASHPs would be positioned on the flat roof of the apartment buildings and on the rear elevation of the houses. Although the Bristol Heat Network does not yet have a connection close to the site, the current proposed heating system for the development would be capable of adaptation to provide for connection to any future networks.

Overheating

The glazing g-value to window glass (the measure of solar heat transmission) would need to be greater than intended 0.4 to mitigate overheating for the 2050 climate scenario (this covers years 2041-2070). It is not proposed that this is installed from the outset, but instead it is assumed that the (initially) installed glazing would necessarily be replaced at the end of its anticipated design life (25yrs i.e. likely in 2046) with glazing that has the requisite g-value of 0.4.

2080

At the time of writing mitigation options for the 2080 climate change scenario have not been modelled. The 2080 shading mitigation measures reasonably envisioned might include external shutters which could be opened and closed as required by future occupants. These shutters could perhaps be integrated as part of any replacement window or be fixed directly to brickwork and would not then require fixing directly back to primary structures.

Proposals are judged satisfactory and a schedule of condition(s) would apply.

(G) WHAT REMEDIAL MEASURES WILL BE TAKEN TO ENSURE THAT THERE IS NO UNACCEPTABLE RISK OF POLLUTION?

Both a contamination desk based assessment and ground investigation has been submitted. The Pollution Control Team is satisfied with the works undertaken to date to include all pollutant linkages; a risk assessment and a written remediation method statement. A Certificate of Remediation' should be provided by the developer upon completion of the works, confirming that the agreed remediation strategy was followed.

AIR QUALITY

Policy DM23 requires any scheme that has the potential for significant emissions to the detriment of air quality should include mitigation measures. The air quality assessment (AQA) submitted with proposals highlights that there are potentially slight adverse impacts upon air quality allied to construction site traffic movement due to the incidence of vehicle emissions. Importantly traffic movements of all types to or from the site at any later date are not, in air quality terms, a concern. Officers are continuing discussion allied to the character of vehicles used, for construction purposes to include their type, size and direction of travel. Matters would be the subject of conditions that would require prior approval if planning permission was granted.

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(H) HAS A DRAINAGE STRATEGY BEEN SUBMITTED AND HOW WILL SURFACE WATER RUN-OFF BE DEALT WITH?

The strategy proposes an innovative range of Sustainable Urban Drainage Systems (SuDS) including bio-retention systems and Stockholm tree pits placed in the highway. The principle of their use is accepted subject to both prior approval of their detailed design and agreement upon relevant commuted sums. A robust management and maintenance arrangement would be required. The strategy also utilises above ground attenuation basins supplemented by below ground attenuation crates. This approach is intended to manage all storms up to the 1 in 100 event including 40% climate change allowance, restricting the outfall to existing Qbar rates (Qbar is the peak rate of flow from a catchment for the mean annual flood). This approach could potentially result in very large storage volumes. As neighbouring land owner Network Rail has no objection to the use of soakaways / attenuation ponds / septic tanks as a chosen means of storm/surface water disposal. The proviso is that these must not be constructed near/within 5 metres of Network Rail's boundary or at any point which could possibly adversely affect the stability of Network Rail's property/infrastructure.

Conditions would apply.

(L) PLANNING OBLIGATIONS

The relevant heads of terms or principal terms need to be agreed prior to planning permission being granted both to ensure that the test of necessity is met and in the interests of transparency.

Affordable Homes

30% provision to be made within the s106 agreement- a further 20% intended beyond the terms of the agreement.

1. Highways and Traffic

These payments are agreed as set out in the description of works and in the recommendation below.

3. Ecology

A commuted sum is required for both upgrade and ongoing maintenance of the SNCI, at the time of writing discussion remains ongoing both over the amount required and the terms of payment. To this end a detailed strategy outlining both the character and duration of works to be undertaken has been submitted.

Fire Hydrants

As set out in the description of works and recommendation below payment agreed as necessary for 5 fire hydrants.

On Site-provision - Open Space/Public realm.

A commuted sum is required for ongoing maintenance. At the time of writing discussion remains ongoing both over the amounts required and the terms of payment. Both the quantum and location of open space is agreed to.

Off Site Allotments

A commuted sum is required for provision off site. At the time of writing discussion is continuing over the amounts required and the terms of payment. Discussion includes a detailed appraisal of the feasibility of the possible use of neighbouring land at neighbouring Dover court small holdings for this

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purpose.

Trees- Bristol Tree Replacement Standard

A payment is required and has been agreed to enable the planting off site of 55 new trees, the intended species, size and location to each to be agreed upon at a later date.

Health Assessment that accompanies
The Health Assessment that accompanies
the drawings Proposed Development
will accommodate an estimated 384
new residents³⁴.

CONCLUSION

The principle of the development is supported by the very longstanding Housing site allocation within in the Adopted Local Plan. The scheme as currently presented provides an opportunity, after years of endeavour by many interested parties, to resolve matters allied to infrastructure upon and neighbouring the site. And to do so for the long term. It would deliver both an enhanced public realm and an appropriate mix of new homes, in terms of tenure and size. New open space is proposed alongside, subject to detail, satisfactory ecological mitigation and tree planting. Proposals would aid the health and wellbeing of those living on or near the site providing greater opportunity to use sustainable and safe methods of transport, cycling and walking.

(I) CIL Community Infra Structure Levy

A total of £1288509.38 would be payable allied to any grant of planning permission

RECOMMENDATION

Officers seek delegated approval to continue discussion and thereafter grant planning permission in one of two circumstances;

- (A) Should the site be sold then subject to compilation, at a later date, of the requisite s106 agreement with the below mentioned- Heads of terms; and a schedule of planning condition(s).
- (B) Should the City Council remain landowner; then acting as both landowner and Local Planning Authority LPA the City Council cannot covenant under a section 106 Agreement. Relevant Planning Practice Guidance (PPG) explains that in such circumstance it is then possible to use a Grampian condition that would prevent the commencement of development until a S106 agreement is completed.

In *exceptional* circumstances a negatively worded condition requiring a planning obligation or other agreement to be entered into *before* certain development can commence may also be appropriate, where there is clear evidence that the delivery of the development would otherwise be placed at serious risk

In Case B among the full schedule of condition(s) the following shall apply

. Grampian Condition requiring completion of a S106 Agreement

No development shall take place or commence on any part of the site save for Enabling Works (as

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defined in condition 1 of this Decision Notice) unless and until all parties with a relevant legal interest in the relevant part of the site have entered into a planning obligation with Bristol City Council and South Gloucestershire District Council pursuant to Section 106 of the Town and Country Planning Act 1990 (as amended) substantially in the form of the draft Section 106 agreement appended to this Decision Notice.

Reason: Development must not commence on any part of the development site until all relevant legal interests in the site are bound under the Section 106 agreement to ensure that the development hereby approved is acceptable in planning terms.”

The following Heads of terms shall apply in either case;

Affordable housing: 30% on site.

Traffic and transport; Contribution towards the installation of a MOVA system at the Filton Avenue/Toronto Road/Bridge Walk signalised junction- £35,000.

The installation of 2 new bus stops on Bonnington Walk- £32,845.

TRO: 2 x TROs (to control parking on paved areas and to ensure clear access for all vehicles, and for waiting restrictions on Bonnington Walk and Landseer Avenue required to protect access) - £11,826.

Travel Plan: Implementation of travel plan - £26,640

.
Tree replacement: - £42,086.55 (£765.21 x 55 trees)

.
Fire hydrants: 5 x new hydrants within the development - £7,500 +VAT

.
Allotments: contribution towards offsite allotment provision –TBC.

SNCI – commuted sum for long term management – TBC

Stockholm Tree Pits- TBC

Contribution to maintenance of Open Space- TBC

A list of planning condition(s) to apply in either case; is given below-this is not intended as an exhaustive list- as changes will be likely to occur (both additions and subtractions) as discussion to resolve the content to the s106 agreement may required.

Traffic and Transport

Highway Works

The applicant will be required to undertake the following highway works through an s278 highway works agreement:

- *Installation of tiger crossing at Bonnington Walk including raised table*
- *Installation of tiger crossing at Constable Road including raised table*
- *Segregated two way cycle route of 155m in length from Dover court Road to the Constable Road National Cycle Network access*
- *Speed bumps along Bonnington Walk from Landseer Avenue to Wordsworth Road*
- *Widening of the access onto Landseer Avenue*
- *Creation of access onto Bonnington Walk*

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- Resurfacing of footway along Bonnington Walk and Landseer Avenue
- Upgrade of street lighting at access points on Bonnington Walk and Landseer Avenue.

A series of intended pre commencement condition(s) are then listed in the advice as follows;

Amended Highway Layout: Plans.

Notwithstanding the approved plans, no development shall take place until Amended Internal Highway Layout Plans have been submitted to and approved in writing by the Local Planning Authority.

The plans must include the following:

*Re-assessment of and adjustments to buildouts, junctions, and carriageways
Inclusion of a lighting design for the Concorde Way cycle route,
Assessment and removal of obstructions along the Concorde Way.*

The development shall be carried out in accordance with the approved plan prior to the occupation of the development and the layout shall remain in accordance with the approved plan unless otherwise agreed in writing with the Local Planning Authority.

B1B Highway works – General Arrangement Plan

No development shall take place until general arrangement plan(s) to a scale of 1:200 showing the following works to the adopted highway has been submitted to and approved in writing by the Local Planning Authority.

- Installation of tiger crossing at Bonnington Walk including raised table*
- Installation of tiger crossing at Constable Road including raised table*
- segregated two way cycle routes of 155m in length from Dover Court Road to the Constable Road*
- National Cycle Network access**
- Speed bumps along Bonnington Walk from Landseer Avenue to Wordsworth Road*
- widening of the vehicular access onto Landseer Avenue*
- Creation of access onto Bonnington Walk*
- resurfacing of footway along Bonnington Walk and Landseer Avenue*
- Upgrade of street lighting at access points on Bonnington Walk and Landseer Avenue*

Where applicable indicating proposals for:

- *Existing levels of the finished highway tying into building threshold levels*
- *Alterations to waiting restrictions or other Traffic Regulation Orders to enable the works*
- *Signing, street furniture, street trees and pits*
- *Structures on or adjacent to the highway*
- *Extent of any stopping up, diversion or dedication of new highway (including all public rights of way shown on the definitive map and statement)*

No development shall take place over the route of any public right of way prior to the confirmation of a Town & Country Planning Act 1990 path diversion/stopping up order.

Prior to occupation these works shall be completed to the satisfaction of the Highway Authority and approved in writing by the Local Planning Authority.

Reason: In the interests of public safety and to ensure that all road works associated with the proposed development are: planned; approved in good time (including any statutory processes); undertaken to a standard approved by the Local Planning Authority and are completed before

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occupation.

B2A Highway to be adopted

No development shall take place until plans to a scale of 1:200 showing the following information has been submitted to and approved in writing by the Local Planning Authority.

- *Long sections*
- *General arrangement*
- *Threshold levels to buildings*
- *Drainage*
- *Structures*
- *Street Lighting*
- *Swept path for two directional movement of an 11.4m long refuse vehicle passing a 4.98m long large saloon car*
- *The Concorde Way cycle route to be built an adoptable standard with detail of lighting and tree protection along the length of Concorde Way to an adoptable standard*
- *How the highway design has been futureproofed and designed for the delivery of on and off-street low, fast and rapid speed Electric Vehicle (EV) charging facilities*
- *How the highway design has been futureproofed and designed to accommodate high speed / alternative broadband communication networks (in addition to BT).*

Prior to occupation detailed technical plans to a scale of 1:200 setting out how the internal access road(s) will be constructed to the Highway Authority's adoptable standard shall be submitted and approved in writing by the Local Planning Authority.

These works shall then be completed to the satisfaction of the Highway Authority and approved in writing by the Local Planning Authority.

Reason: To ensure the internal roads are planned and approved in good time to a satisfactory standard for use by the public and are completed prior to occupation.

Construction Management Plan – Major Developments

No development shall take place, including any demolition works, until a construction management plan or construction method statement has been submitted to and approved in writing by the Local Planning Authority. The approved plan/statement shall be adhered to throughout the demolition/construction period. The plan/statement shall provide for:

- *A construction programme including phasing of works;*
- *24 hour emergency contact number;*
- *Hours of operation;*
- *Expected number and type of vehicles accessing the site:*
 - . Deliveries, waste, cranes, equipment, plant, works, visitors;*
 - O Size of construction vehicles;*
 - O The use of a consolidation operation or scheme for the delivery of materials and goods;*
 - O Phasing of works;*
- *Means by which a reduction in the number of movements and parking on nearby streets can be achieved (including measures taken to ensure satisfactory access and movement for existing occupiers of neighbouring properties during construction):*
 - O Programming;*
 - O Waste management;*
 - O Construction methodology;*

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- O Shared deliveries;*
- O Car sharing;*
- O Travel planning;*
- O Local workforce;*
- O Parking facilities for staff and visitors;*
- O On-site facilities;*
- O A scheme to encourage the use of public transport and cycling;*
- Routes for construction traffic, avoiding weight and size restrictions to reduce unsuitable traffic on residential roads;*
- Locations for loading/unloading, waiting/holding areas and means of communication for delivery vehicles if space is unavailable within or near the site;*
- Locations for storage of plant/waste/construction materials;*
- Arrangements for the turning of vehicles, to be within the site unless completely unavoidable;*
- Arrangements to receive abnormal loads or unusually large vehicles; ensure adequate space is available;*
- Any necessary temporary traffic management measures;*
- Measures to protect vulnerable road users (cyclists and pedestrians);*
- Arrangements for temporary facilities for any bus stops or routes;*
- Method of preventing mud being carried onto the highway;*
- Methods of communicating the Construction Management Plan to staff, visitors and neighbouring residents and businesses.*

Reason: In the interests of safe operation of the adopted highway in the lead into development both during the demolition and construction phase of the development.

B39 Highway Condition Survey

No development shall take place (including investigation work, demolition, siting of site compound/welfare facilities) until a survey of the condition of the adopted highway has been submitted to and approved in writing by the Local Planning Authority. The extent of the area to be surveyed must be agreed by the Highways Authority prior to the survey being undertaken.

The survey must consist of:

- A plan to a scale of 1:1000 showing the location of all defects identified;*
- A written and photographic record of all defects with corresponding location references accompanied by a description of the extent of the assessed area and a record of the date, time and weather conditions at the time of the survey.*

No building or use hereby permitted shall be occupied or the use commenced until any damage to the adopted highway has been made good to the satisfaction of the Highway Authority.

Reason: To ensure that any damage to the adopted highway sustained throughout the development process can be identified and subsequently remedied at the expense of the developer.

B43 Temporary Access to the Site

No development shall take place until a plan that shows any temporary access from the adopted highway and the routes construction traffic will use has been submitted to and approved in writing by the Local Planning Authority.

Reason: In the interest of highway safety

Thereafter a series of regulatory conditions are also advised and listed as follows;

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Car Park Management Plan

No development shall take place until a car park management plan setting out how the car parking will be managed has been prepared, submitted to and approved in writing by the Local Planning Authority. The measures shall thereafter be implemented in accordance with the approved car park management plan for the lifetime of the development.

Reason: To ensure the safe operation of approved car park(s).

Electric Vehicle Charging Points

No development shall take place until full details of the proposed Electrical Vehicle Charging facilities to serve the development have been submitted to and approved in writing by the Local Planning Authority.

This shall include passive provision for 80% of all in-curtilage parking spaces and active provision for 20% of all on-street parking spaces which should also include fast as well as rapid charging points. Ducting and associated power connections, including transformer units shall be provided throughout the development as required to enable charging points to be installed to all parking spaces at a future date. The development shall be implemented in accordance with that approval prior to occupation of the development and all charging facilities thereafter retained.

Reason; In order to enable and encourage lower polluting forms of vehicular fuel in the interests of air quality

C5A Implementation/Installation of Refuse Storage and Recycling Facilities – Shown on approved plans

No building or use hereby permitted shall be occupied or use commenced until the refuse store and area/facilities allocated for storing of recyclable materials, as shown on the approved plans have been completed in accordance with the approved plans. Thereafter, all refuse and recyclable materials associated with the development shall either be stored within this dedicated store/area, as shown on the approved plans, or internally within the building(s) that form part of the application site. No refuse or recycling material shall be stored or placed for collection on the adopted highway (including the footway), except on the day of collection.

Reason: To safeguard the amenity of the occupiers of adjoining premises; protect the general environment; prevent any obstruction to pedestrian movement and to ensure that there are adequate facilities for the storage and recycling of recoverable materials.

C8 Completion of Pedestrians/Cyclists Access – Shown on approved plans

No building or use hereby permitted shall be occupied or the use commenced until the means of access for pedestrians and/or cyclists have been constructed in accordance with the approved plans and shall thereafter be retained for access purposes only.

Reason: In the interests of highway safety.

C11 Completion and Maintenance of Vehicular Servicing facilities – Shown on approved plans

No building or use hereby permitted shall be occupied or use commenced until the facilities for loading, unloading, circulation and maneuvering have been completed in accordance with the

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approved plans. Thereafter, these areas shall be kept free of obstruction and available for these uses.

Reason: To ensure that there are adequate servicing facilities within the site in the interests of highway safety.

C12A Completion and Maintenance of Car/Vehicle Parking – Shown on approved plans

No building or use hereby permitted shall be occupied or use commenced until the car/vehicle parking area (and turning space) shown on the approved plans has been completed and thereafter the area shall be kept free of obstruction and available for the parking of vehicles associated with the development. Driveways/vehicle parking areas accessed from the adopted highway must be properly consolidated and surfaced, and be subsequently maintained in good working order at all times thereafter for the lifetime of the development.

Reason: To ensure that there are adequate parking facilities to serve the development constructed to an acceptable standard.

C13 Completion and Maintenance of Cycle Provision – Shown on approved plans

No building or use hereby permitted shall be occupied or the use commenced until the cycle parking provision shown on the approved plans has been completed, and thereafter, be kept free of obstruction and available for the parking of cycles only.

Reason: To ensure the provision and availability of adequate cycle parking

C29 Management and Maintenance of Private Streets

No building or use hereby permitted shall be occupied or use commenced until details of arrangements for the future management and maintenance of proposed carriageways, footways, footpaths and landscaped areas not put forward for adoption within the site has been submitted to and approved in writing by the Local Planning Authority. Following occupation of the first dwelling on the site, the streets shall be maintained in accordance with the approved management and maintenance details.

Reason: To ensure that all private streets and landscaped areas are appropriately managed and maintained to ensure the safety of all users.

C35 Car Club

No building or use hereby permitted shall be occupied or use commenced until details of a car club scheme, in accordance with a contract to be entered into by the developer and an approved car club provider, shall be submitted to and approved in writing by the Local Planning Authority. The car club scheme shall comprise (where applicable):

- The allocation of one car club parking space*
- The provision of one electric vehicle*
- The provision of a rapid EV charging bay*
- Provision of car club membership for all eligible residents of the development for a minimum of three years*
- Promotion of the scheme*
- The phasing at which the scheme will be introduced*

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D37 Travel Plan – Implemented by the Highways Authority

Prior to occupation or use commenced, evidence that the pre-occupation elements of the approved Travel Plan have been put in place shall be prepared, submitted to and approved in writing by the Local Planning Authority. The developer shall then enable the Highways Authority to implement, monitor and review the approved Travel Plan to the satisfaction of Local Planning Authority unless agreed in writing by the Local Planning Authority.

Reason: To support sustainable transport objectives including a reduction in single occupancy car journeys and the increased use of public transport, walking and cycling.

Ecology

These may well be subject to necessary change as translocation of species referred to is at or near completion-monitoring would continue.

Condition: Prior to the commencement of development hereby approved, including all site clearance and vegetation removal, an ecological mitigation and enhancement strategy prepared by a suitably qualified ecological consultant, shall be submitted to and approved in writing by the Local Planning Authority. This shall include:

- *A Precautionary Method of Working (PMW) method statement with respect to the potential presence of legally protected and priority species to include nesting birds, badgers and hedgehogs. An update check for badger setts shall be undertaken no more than 48 hours prior to the commencement of works or vegetation clearance. The PMW shall include precautionary measures to protect badgers and other animals becoming trapped in open trenches or pipework. Where checks for nesting birds are required they shall be undertaken by a qualified ecological consultant no more than 48 hours prior to the removal of vegetation or the demolition of, or works to buildings;*
- *Details of the provision of bird, bat, insect and hedgehog boxes or bricks (to include provision for house sparrows) to include the location, specification, height and orientation of these features as applicable shown on a site plan;*
- *A method statement for the control of invasive plants including Japanese knotweed, variegated yellow archangel and Wall cotoneaster;*
- *Details of an interpretation board to be placed adjacent to the SNCI to explain the importance of the site and its specific management for reptiles, grassland and other wildlife;*
- *The provision of biodiverse living roofs (avoiding the use of Sedum) on cycle shelters, bin roofs or other flat roofs;*
- *An external lighting lux contour plan which extends outwards to levels of zero lux superimposed on a site plan;*
- *A ten year nature conservation and landscape management plan for the non SNCI part of the site (a separate SNCI management strategy has been submitted separately).*

The development shall be carried out in full accordance with the approved strategy.

Reason: To conserve legally protected and priority (Section 41) species which are a material planning consideration and provide ecological mitigation in accordance with paragraph 170(d) of the National Planning Policy Framework (2018).

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Condition: No more than a week prior to the commencement of development, a check for active fox earths (to include partially active badger setts) shall be undertaken on the site by a qualified ecological consultant. If an active fox earth is found then an appropriate mitigation strategy shall be submitted to and approved in writing by the local planning authority. The development shall be carried out in accordance with the approved mitigation strategy.

Reason: To conform to the legal requirements of the Wild Mammals (Protection) Act 1996.

Building and Landscape Design; *Standard condition(s) seeking the following are required;*

Typical details of following elements of the building(s) design;

- *Plinth*
- *Door and Window Reveals, Lintel and Sills*
- *Canopies*
- *Eaves, parapet, Ridge, barge including arrangement of Rain Water Goods*
- *Construction of the structure including arrangement and fixing of external cladding*
- *Junction between panels of the same materials and junction between different materials in the same plane and at corners*
- *Details of charges to the brickwork and coursing*

Considering the chosen off-site construction method, it is recommended that prior to commencement of above ground works be used as the trigger point.

Panel(s) presenting all external building finishing materials to be erected on site and approved; In view of the chosen means of construction (off-site construction method), it is recommended to include prior to commencement of above ground works as a trigger point for this condition.

A landscape management plan; which better clarifies the intended management arrangement for the site as whole (i.e.; the responsibilities of each owner; (it should be noted that there would be an intent to remove permitted development right(s) for individual owners (dwelling houses Use class C3) in the interests of preserving both visual and residential amenity.

Typical arrangement for securing the alleyways providing access to rear garden both in terms of design and management.

Condition(s); seeking all materials for landscaping to cover all hard surfacing, kerbs, borders, fencing and railings.

A landscape management plan; which confirms the intended management arrangement for the site as whole (i.e.; the responsibilities of each owner; (it should be noted that there would be an intent to remove permitted development right(s) for individual owners (dwelling houses Use class C3) in the interests of preserving both visual and residential amenity.

Typical arrangement for securing the alleyways providing access to rear garden both in terms of design and management.

Condition(s); seeking all materials for landscaping to cover all hard surfacing, kerb(s), borders, fencing and railings.

Standard condition seeking typical details of tree planting pits, pits for planting areas and soft landscaping”

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Arboricultural-Tree planting

1. Protection of Retained Trees during the Construction Period

No work of any kind shall take place on the site until the protective barriers have been erected around the retained trees, in the position and to the specification shown on Drawing No. XXXXXXXXXXXXX. Once installed photos are, to be verified in writing by the Tree Officer. The Local Planning Authority shall be given not less than two weeks prior written notice by the developer of the commencement of works on the site in order that the council may verify in writing that the approved tree protection measures are in place when the work commences. The approved fence(s) shall be in place before any equipment, machinery or materials are brought on to the site for the purposes of the development and shall be maintained until all equipment, machinery and surplus materials have been removed from the site. Within the fenced area(s) there shall be no scaffolding, no stockpiling of any materials or soil, no machinery or other equipment parked or operated, no traffic over the root system, no changes to the soil level, no excavation of trenches, no site huts, no fires lit, no dumping of toxic chemicals and no retained trees shall be used for winching purposes. If any retained tree is removed, uprooted or destroyed or dies, another tree shall be planted at the same place and that tree shall be of such size and species, and shall be planted at such time, as may be specified in writing by the council.

Under no circumstances should the tree protection be moved during the period of the development and until all works are completed and all materials and machinery are removed. Landscaping works within protected areas is to be agreed with the Local Planning Authority and carried out when all other construction and landscaping works are complete.

Reason: To protect the retained trees from damage during construction, including all ground works and works that may be required by other conditions, and in recognition of the contribution which the retained tree(s) give(s) and will continue to give to the amenity of the area in line with Policy DM17.

Arboricultural Supervision

Prior to the commencement of development, a pre-commencement site meeting shall be held and attended by the developer's Arboricultural consultant and the designated site foreman to discuss details of the working procedures. A schedule of visits shall be drawn up to ensure the Project arboriculturist is present during key stages of the development which include, but not limited to:

- X
- X
- X

Site visits must be carried out during the key stages identified above. Copies of written site notes and/or reports detailing the results of site supervision and any necessary remedial works undertaken or required shall be submitted to and approved in writing by the Local Planning Authority, prior to occupancy. Any approved remedial works shall subsequently be carried out under strict supervision by the Arboricultural consultant immediately following that approval.

Reason: In order that the Local Planning Authority may be satisfied that the trees to be retained on-site will not be damaged during the construction works and to ensure that as far as possible the work is carried out in accordance with current best practice.

Landscape (Soft and Hard) – (Major applications)

Prior to completion or first occupation of the development hereby approved, whichever is the sooner; details of treatment of all parts on the site not covered by buildings shall be submitted to and approved in writing by the Local Planning Authority. The site shall be landscaped strictly in accordance with the approved details in the first planting season after completion or first occupation of

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the development, whichever is the sooner. Details shall include:

- 1) *a scaled plan showing all existing vegetation and landscape features to be retained and trees and plants to be planted;*
- 2) *location, type and materials to be used for hard landscaping including specifications for:*
 - a. permeable paving*
 - b. underground modular systems*
 - c. Soil aeration vents*
 - d. Soil type and soil volumes available for each tree*
 - e. Sustainable urban drainage integration*
 - f. use within tree Root Protection Areas (RPAs);*
- 3) *a schedule detailing sizes and numbers/densities of all proposed trees/plants;*
- 4) *specifications for operations associated with plant establishment and maintenance that are compliant with best practise; and*
- 5) *types and dimensions of all boundary treatments*

There shall be no excavation or raising or lowering of levels within the prescribed root protection area of retained trees unless agreed in writing by the Local Planning Authority. Unless required by a separate landscape management condition, all soft landscaping shall have a written five year maintenance programme following planting. Any tree(s) that die(s), are/is removed or become(s) severely damaged or diseased shall be replaced and any new planting (other than trees) which dies, is removed, becomes severely damaged or diseased within five years shall be replaced. Unless further specific permission has been given by the Local Planning Authority, replacement planting shall be in accordance with the approved details.

Reason: Required to safeguard and enhance the character and amenity of the area, to provide ecological, environmental and bio-diversity benefits and to maximise the quality and usability of open spaces within the development, and to enhance its setting within the immediate locality in accordance with DM15 and DM17.

Woodland management plan

Prior to completion or first occupation of the development hereby approved, whichever is the sooner; a 25 year woodland management plan shall be submitted to, and approved in writing, by the Local Planning Authority. The management plan should be prepared by a qualified and experienced forestry or Arboricultural consultant and should include the following elements:

- a) A Statement of the overall design vision for the woodland and for individual trees retained as part of the development – including amenity classification, nature conservation and accessibility.*
- b) Type and frequency of management operations to achieve and sustain canopy, understorey and ground cover, and to provide reinstatement including planting where tree loss or vandalism occurs.*
- c) Frequency of safety inspections, which should be at least three yearly in areas of high risk, less often in lower risk areas.*
- d) Confirmation that the tree pruning work is carried out by a suitably qualified and insured tree contractor to British Standard 3998 (2010).*
- e) Special measures relating to protected species or habitats, e.g. intensive operations to avoid March – June nesting season or flowering period.*
- f) Inspection for pests, vermin and diseases and proposed remedial measures.*
- g) Recommendations relating to how trees within the immediate vicinity of properties or within private areas are to be protected, such that these are retained areas are to be protected such that these are retained without the loss of their canopy or value as habitat.*

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- h) *Confirmation of cyclical management plan assessments and revisions to evaluate the plan's success and identification of any proposed actions.*

Reason: Required to ensure that woodland areas are satisfactorily safeguarded, managed and maintained in the long term/ in perpetuity in the interest of nature conservation and the visual amenity of the area and to accord with policies DM15, DM 19 of the Bristol Core Strategy BCS9 and the NPPF.

Public Art

To follow

Public Protection Ground Contamination**B11 - Amended. Phase 2 site investigation**

A site specific risk assessment and intrusive investigation shall be carried out to assess the nature and extent of any site contamination and whether or not it originates from the site. The investigation and risk assessment must be undertaken by competent persons and a written report of the findings must be produced. The results of this investigation shall be considered along with the reports submitted with the original application. The written report of the findings shall be submitted to an approved in writing by the Local Planning Authority prior to any works (except demolition) in connection with the development, hereby approved, commencing on site. This must be conducted in accordance with the Environment Agency's 'Land Contamination: risk management' and BS 10175:2011 + A2:2017: Investigation of Potentially Contaminated Sites - Code of Practice.

B12 - Submission of Remediation Scheme

No development shall take place until a detailed remediation scheme to bring the site to a condition suitable for the intended use by removing unacceptable risks to human health, buildings and other property and the natural and historical environment has been prepared, submitted to and approved in writing by the Local Planning Authority. The scheme must include all works to be undertaken, proposed remediation objectives and remediation criteria, timetable of works and site management procedures. The scheme must ensure that the site will not qualify as contaminated land under Part 2A of the Environmental Protection Act 1990 in relation to the intended use of the land after remediation.

B13 - Implementation of Approved Remediation Scheme

In the event that contamination is found, no occupation of the development shall take place until the approved remediation scheme has been carried out in accordance with its terms. The Local Planning Authority must be given two weeks written notification of commencement of the remediation scheme works.

Following completion of measures identified in the approved remediation scheme, a verification report (otherwise known as a validation report) that demonstrates the effectiveness of the remediation carried out must be produced, and be approved in writing by the Local Planning Authority.

C1: Reporting of Unexpected Contamination

In the event that contamination is found at any time that had not previously been identified when carrying out the approved development, it must be reported immediately to the Local Planning Authority. An investigation and risk assessment must be undertaken in accordance with the Environment Agency's 'Land Contamination: risk management' guidance and BS 10175:2011 + A2:2017: Investigation of Potentially Contaminated Sites - Code of Practice. Where remediation is necessary a remediation scheme must be prepared which ensures the site will not qualify as

Development Control – 25th November 2020

Application No. 20/02523/PB: Land on South Side of Bonnington Walk Lockleaze Bristol

contaminated land under Part 2A of the Environmental Protection Act 1990 in relation to the intended use of the land after remediation.

Following completion of measures identified in the approved remediation scheme a verification report must be prepared, which is subject to the approval in writing of the Local Planning Authority. The Local Planning Authority must be given two weeks written notification of commencement of the remediation scheme works.

For all conditions:

Reason: To ensure that risks from land contamination to the future users of the land and neighbouring land are minimised, together with those to controlled waters, property and ecological systems, and to ensure that the development can be carried out safely without unacceptable risks to workers, neighbours and other offsite receptors.

Pollution Control (Noise)

1. Construction Management Plan

No development shall take place until a site specific Construction Management Plan has been submitted to and approved in writing by the Council. The plan must demonstrate the adoption and use of the best practicable means to reduce the effects of noise, vibration, dust and site lighting.

Advice

The Construction Environmental Management Plan should also include but is not limited to reference to the following:

- *All works and ancillary operations which are audible at the site boundary, or at such other place as may be agreed with the Local Planning Authority, shall be carried out only between the following hours: 08 00 Hours and 18 00 Hours on Mondays to Fridays and 08 00 and 13 00 Hours on Saturdays and at no time on Sundays and Bank Holidays.*
- *Mitigation measures as defined in BS 5528: Parts 1 and 2: 2009 Noise and Vibration Control on Construction and Open Sites shall be used to minimise noise disturbance from construction works.*
- *Procedures for emergency deviation of the agreed working hours.*
- *Control measures for dust and other air-borne pollutants.*
- *Measures for controlling the use of site lighting whether required for safe working or for security purposes.*

2. Sound insulation of residential properties from external noise

All recommendation detailed in the Noise Assessments submitted with the application with regards to sound insulation and ventilation of residential properties shall be implemented in full prior to the commencement of the use permitted and be permanently maintained.

Sustainability

These remain under discussion

Flood Risk Management

These remain under discussion

Supporting Documents

1. Land On South Side Of Bonnington Walk.

1. Masterplan
2. Aerial Street Views
3. Appearance



PO1	05.06.20	PLANNING ISSUE - FINAL FOR SUBMISSION
PO2	01.05.20	PLANNING ISSUE - FINAL DRAFT
PO1	13.03.20	PLANNING ISSUE - DRAFT
REV	DATE	

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LOCATION


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6.0 Scale & massing

6.3 Aerial street views



6.0 Scale & massing

6.3 Aerial street views



6.0 Scale & massing

6.3 Aerial street views



6.0 Scale & massing

6.3 Aerial street views

Rear gardens.
Timber fences with
concrete fence posts.
Gaps in gravel boards
for wildlife

Terraced
housing

On street
parking

Apartment building
Photovoltaic panels
& air source heat
pumps



Existing trees
retained

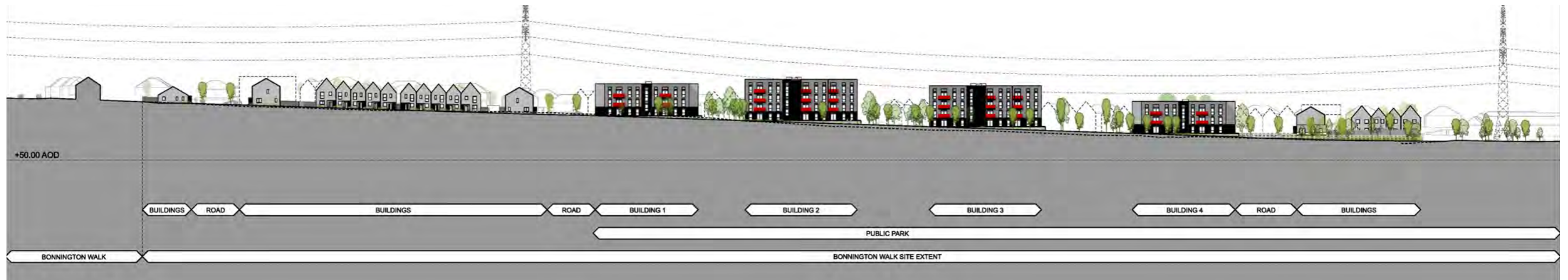
Secure access to
back gardens

Tiled roofs with
Photovoltaic
panels

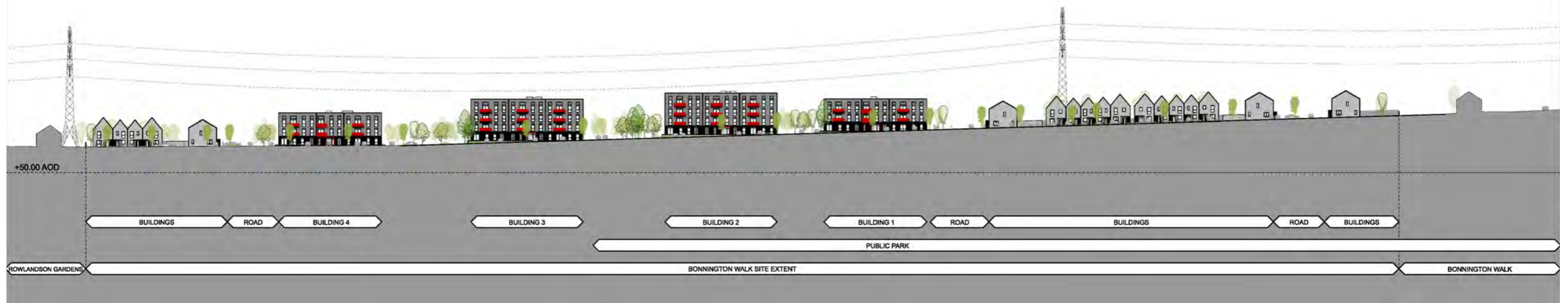
New street trees

6.0 Scale & massing

6.4 Site sections



North-south site section looking east



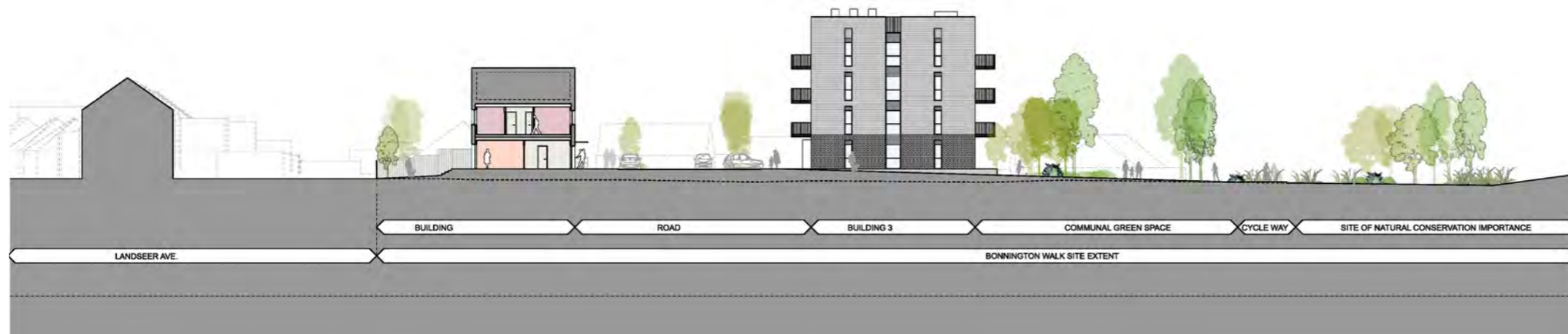
North-south site section looking west

6.0 Scale & massing

6.4 Site sections



East-west site section looking south



East-west site section looking south

7.0 Appearance

7.1 Street views



View from cycle path looking south over new public open space

7.0 Appearance

7.2 House elevations & materials

House elevations have been developed as a simple and timeless design. Brick has been chosen as the primary material being both robust and low maintenance, with a long design life.

Two main bricks have been proposed - a pale, textured brick with colour variation for the pitched gable houses and a complementary mid-tone brick, also with texture and colour variation for the front-to-back pitched roof houses. Each house would have a low brick wall to their defensible space in the same brick type.

A highlight brick has been placed around front doors and vertical recesses between dwellings to signify the entrance and create identity and character to the houses.

All materials and colours TBA



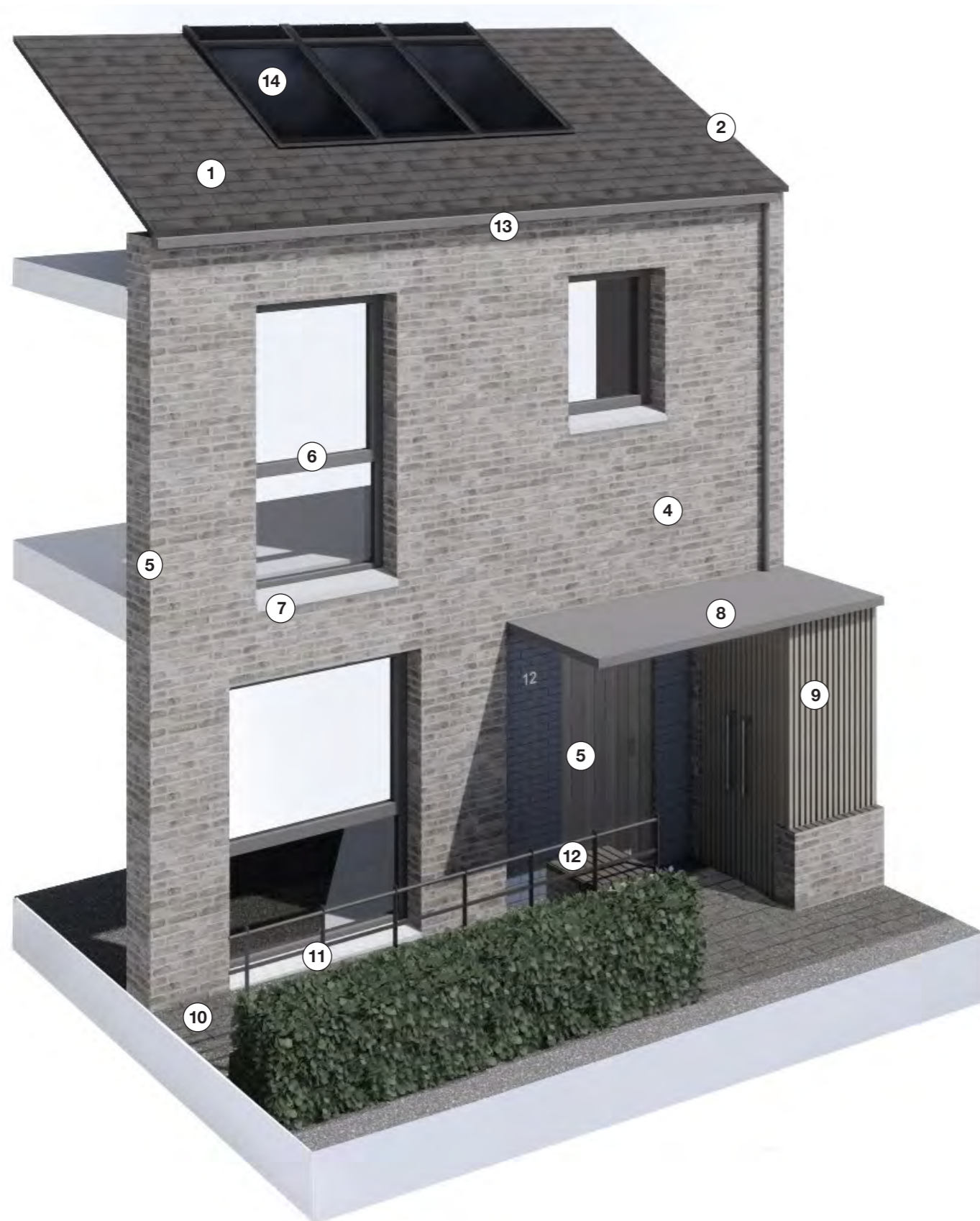
Examples of AHMM pitched gable roof projects



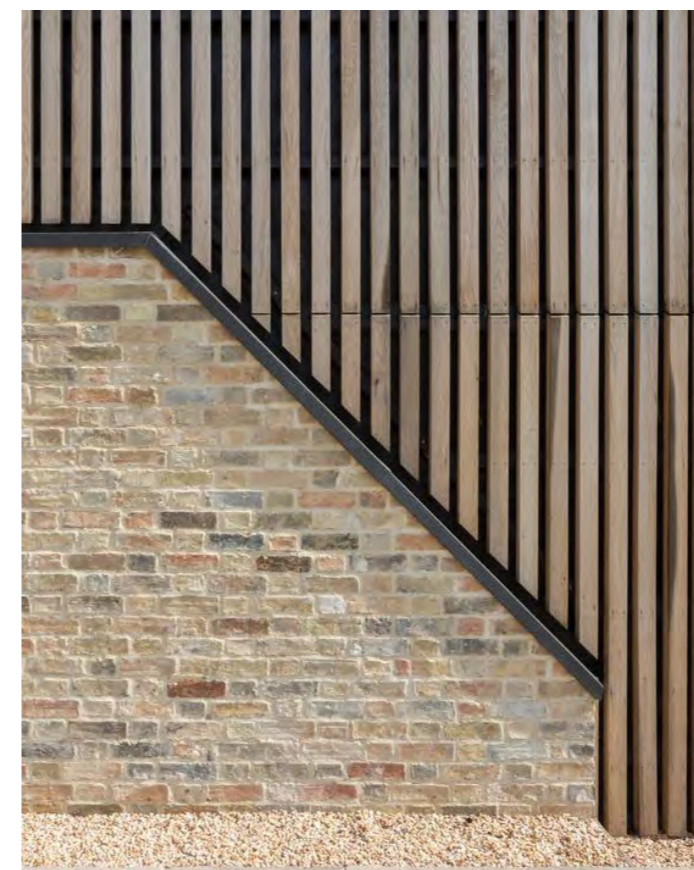
Bay study of pitched gable roof house type

7.0 Appearance

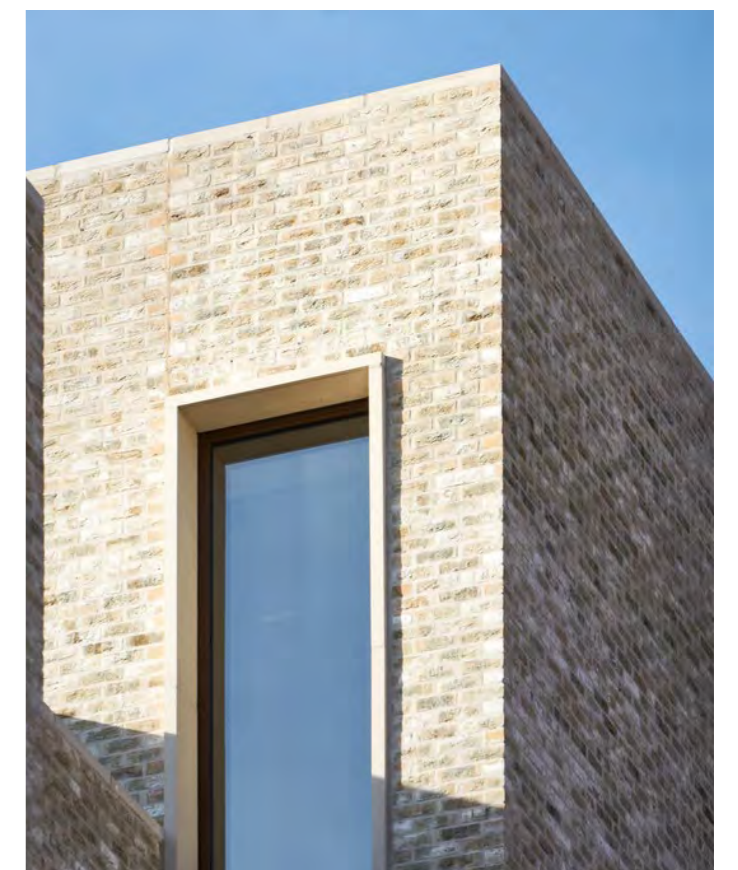
7.2 House elevations & materials



Bay study of front-to-back roof house type



- ① Artificial slate roofs
- ② Powder coated aluminium roof trims
- ③ Brick type A: Pale, textured brick: variation in colour
- ④ Brick type B: Mid-tone textured brick: variation in colour
- ⑤ Brick type C: Feature highlight brick to door and recess
- ⑥ Grey UPVC windows
- ⑦ Pre-cast window sill
- ⑧ Storage porch for refuse and recycling
- ⑨ Stained vertical timber cladding
- ⑩ Defensible space
- ⑪ Painted metal railing behind hedge
- ⑫ Brickwork bench with timber seat
- ⑬ Colour matched rainwater goods
- ⑭ Roof mounted photovoltaic panels, size and location to be determined



7.0 Appearance

7.3 Apartment elevations & materials

Apartment elevations have been developed to compliment the houses, using the same brick types. Upper floors use the pale brick, but sit upon a single storey ground floor base that uses the mid-tone brick mixed with the pale brick in a Flemish bond header coursing pattern.

A vertical recess using the highlight brick delineates the party walls between individual dwellings stacked upon one another.

Upper floor windows have a simple recessed brick panel extending above the opening to give the top of the buildings additional scale

Balconies use a flat vertical bar steel balustrade running past the deck fascia.

All materials and colours TBA

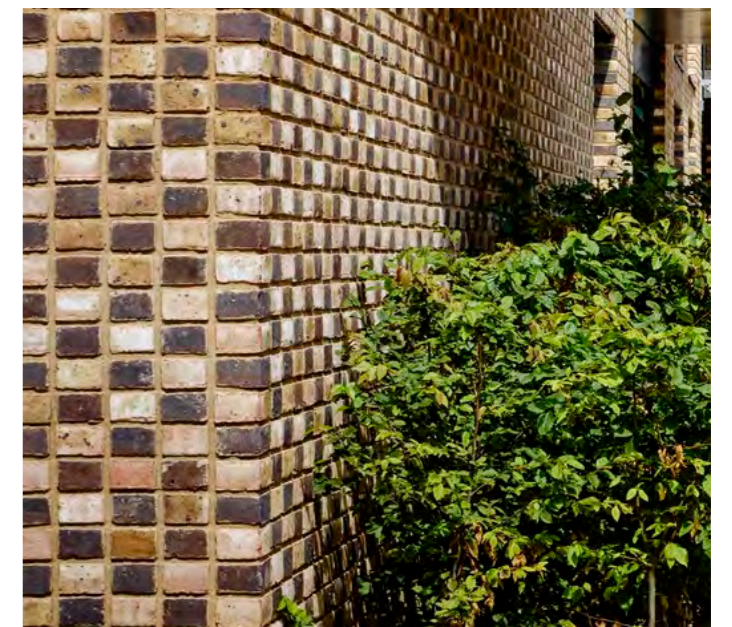
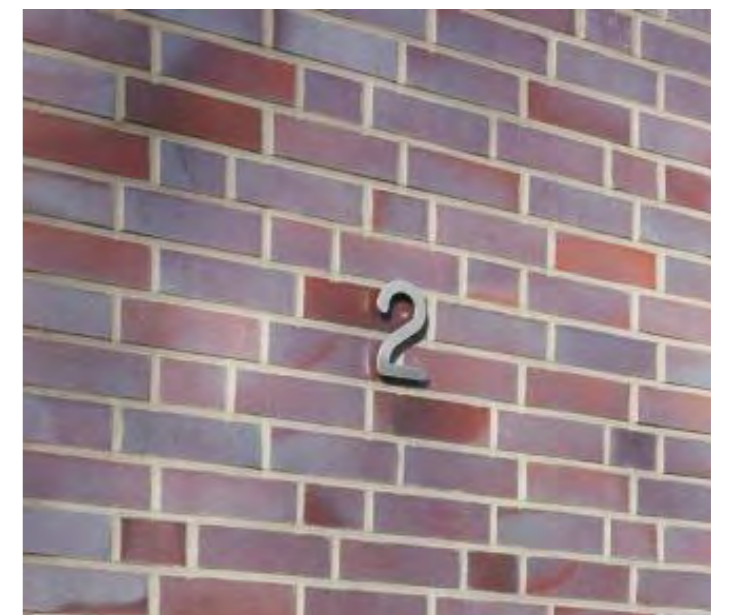


7.0 Appearance

7.3 Apartment elevations & materials



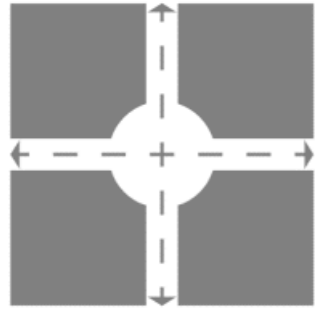
- ① Powder coated metal coping to parapet
- ② Brick type A: Pale, textured brick: variation in colour
- ③ Brick type B: Mid-tone textured brick: variation in colour
- ④ Brick type C: Feature brick highlight to recessed entrance and vertical recess between dwellings
- ⑤ Bonded brickwork: Mix of Brick A and B with Flemish bond header course
- ⑥ Pre-cast window sill
- ⑦ Defensible space to ground floor apartments
- ⑧ Vertical steel flat bar powder coated balcony balustrade and balcony fascia.
- ⑨ Spandrel panel above windows
- ⑩ Plant room / cycle store door
- ⑪ Photovoltaic panels to roof (indicative layout)
- ⑫ Recess brick panel above upper floor windows
- ⑬ Grey UPVC windows



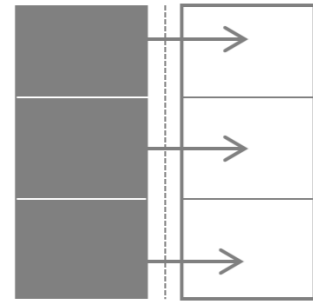
Bay study of apartment building

7.0 Appearance

7.4 Key design ideas



Simple, legible urban grain



Repairing boundaries



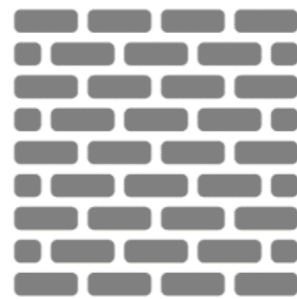
Enhanced wildlife corridor



Efficient, repeating dwelling typologies



Characterful landscape & public open space



Simple, robust palette of materials



Varied roof scape



Frontage to street & cycle path

7.0 Appearance

7.5 Street views



View from east avenue looking north

